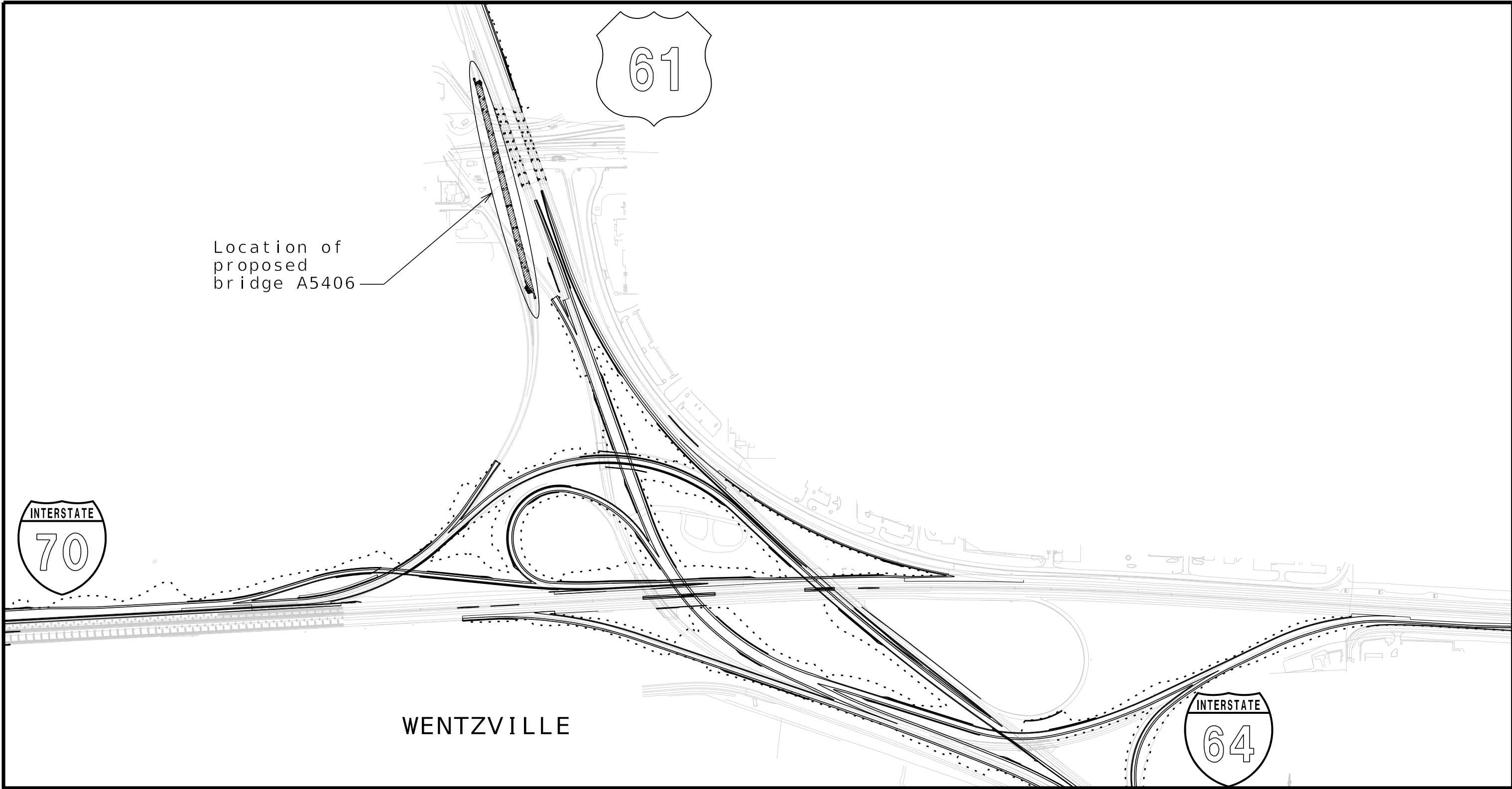


U.I.P. AND REHABILITATE (69' - 69' - 120' - 108' - 104' - 90') (90' - 120' - 91' - 120' - 116') CONTINUOUS PRESTRESSED CONCRETE BULB TEE SPANS



LOCATION MAP

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A54062 - 6	DETAILS OF REPAIRS AT BENTS 4 THRU 7
A54062 - 7	DETAILS OF REPAIRS AT BENTS 10 & 11
A54062 - 8	DETAILS OF REPAIRS AT SUPERSTRUCTURE
A54062 - 9	DETAILS OF STOPPER PLATES
A54062 - 10	SLAB DEMOITION AND REPLACEMENT AT END BENT 1 & 12
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A54062 - 16	BRIDGE APPROACH SLAB REPAIR
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A54062 - 18	BILL OF REINFORCING
A54062 - 19	SURVEY CONTROL POINTS
A54062 - 20	SURVEY CONTROL DATA

BRIDGE: RAMP 61S TO 70W OVER PEARCE BLVD.,
NORFOLK SOUTHERN RAILROAD,
PITMAN AVE. AND OLD RTE. 61

FROM RTE. A TO I-70 US 61 INTERCHANGE
IN WENTZVILLE

Designed JUN 2025
Detailed JUN 2025
Checked JUN 2025

STATE OF MISSOURI

David J. Glasstetter

NUMBER
PE-2000150018

PROFESSIONAL ENGINEER

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ROUTE
I - 70

STATE
MO

DISTRICT
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SHEET NO.
A54062-1

COUNTY
ST. CHARLES

JOB NO.
JST0020

CONTRACT ID.

PROJECT NO.

BRIDGE NO.
A54062

DESCRIPTION	DATE

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105 WEST CAPITOL
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IMPROVE 70 ALLIANCE

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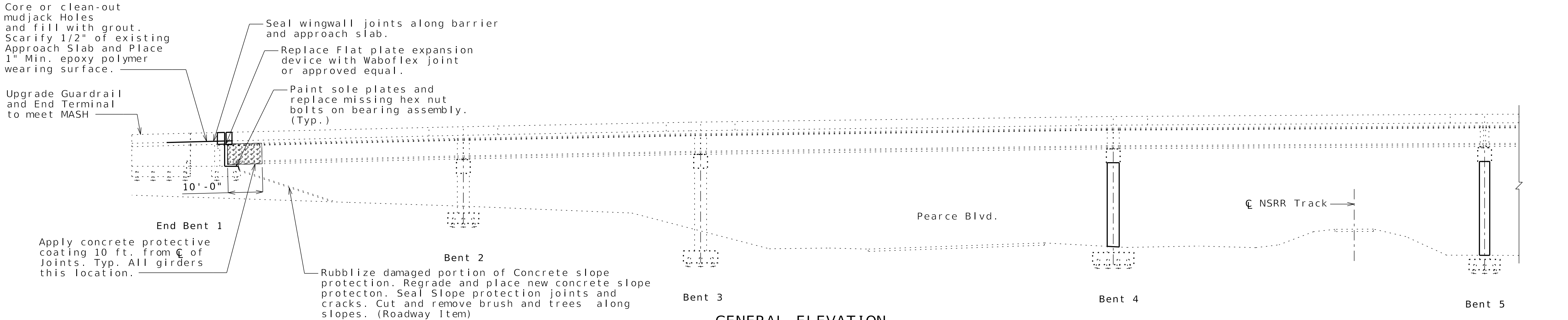
715 KIRK DRIVE
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CERTIFICATE OF AUTHORITY
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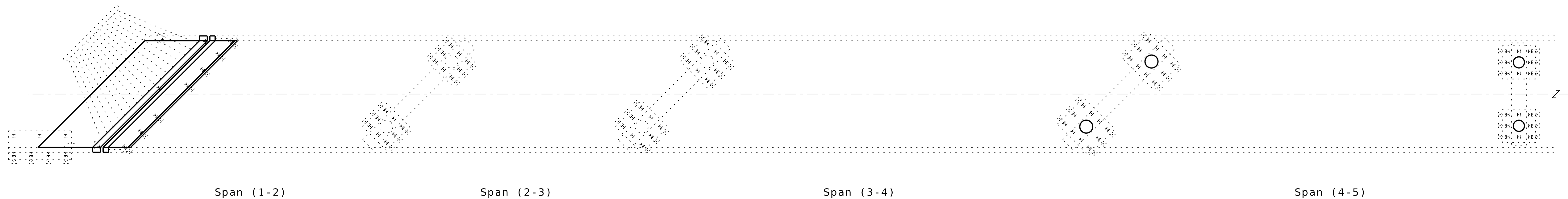
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ENGINEERING
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U.I.P AND REHABILITATE (69' - 69' - 120' - 108' - 104' - 90') (90' - 120' - 91' - 120' - 116') CONTINUOUS PRESTRESSED CONCRETE BULB TEE SPANS



GENERAL ELEVATION



PLAN

General Notes:

Existing Design Loadings:

HS20 Modified
Military 24,000# Tandem Axle
Earth = 120#/cu. ft.
Equivalent Fluid pressure = 51#/cu.ft.
(Calculated from a Phi Angle = 24°)
35#/sq. ft. Future Wearing Surface
Superstructure: Simply supported, Non-composite for Dead Load. Continuous Composite for Live Load.

Design Unit Stresses:

Class B-1 Concrete (Barrier) f'c = 4,000 psi
Class B-2 Concrete (Superstructure) f'c = 4,000 psi
Reinforcing Steel (ASTM A706 Grade 60) fy = 60,000 psi

Concrete for repairing concrete deck shall be a qualified special mortar in accordance with Sec 704 instead of the Class B-2 or B-1 Concrete.

Roadway surfacing adjacent to bridge ends shall match new bridge wearing surface (roadway item)

Outline of existing work is indicated by light dashed line. Heavy lines indicate new work.

Contractor shall verify all dimensions before ordering new materials

Traffic Handling:

Structure to be closed during construction. See Maintenance of Traffic plans for traffic control.

Concrete Protective Coating:

Protective coating for concrete bents and piers (Epoxy) shall be applied as shown on the bridge plans and in accordance with Sec 711.

Cleaning and Protective Coating (Epoxy) shall be applied as shown on the bridge plans and in accordance with Job Special Provisions.

Structural Steel Protective Coatings:

Coating limits: Anchor bolts, stopper plate and associated hardware, and sole plates of bearings, shall be coated with one 6-mil thickness of gray epoxy mastic primer applied over an SSPC-SP3 surface preparation in accordance with Sec 1081.

Reinforcing Steel:

All reinforcing steel used in reconstruction of the slab and barrier shall be epoxy coated.

Minimum clearance to reinforcing steel shall be 1-1/2", unless otherwise shown.

Minimum clearance to epoxy grouted bars per manufacturer's recommendation.

GENERAL PLAN AND ELEVATION (1 OF 3)

Detailed JUN 2025
Checked JUN 2025

Note: This drawing is not to scale. Follow dimensions.

Sheet 2 of 20

STATE OF MISSOURI
David J. Glastetter
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ROUTE
1 - 70
STATE
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DISTRICT
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SHEET NO.
A54062-2

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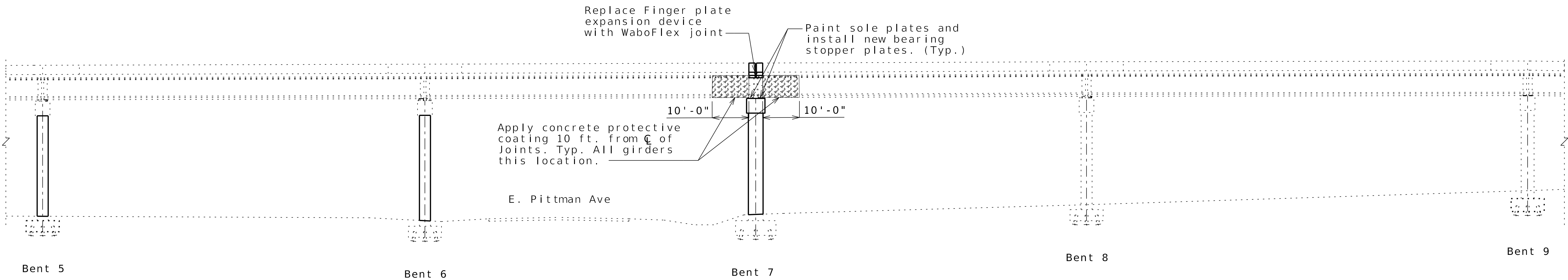
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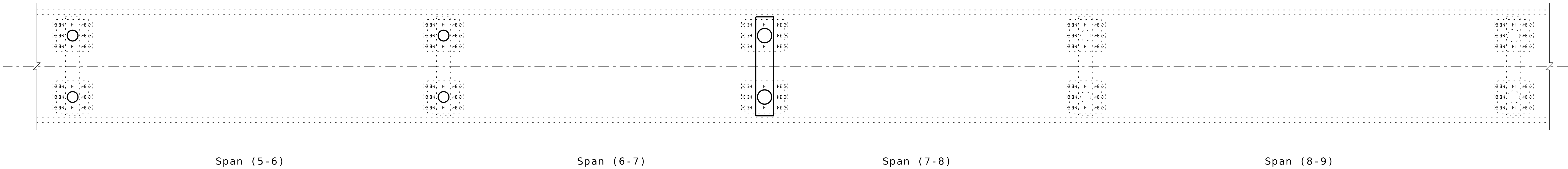
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GENERAL ELEVATION



PLAN

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Sheet 3 of 20

GENERAL PLAN AND ELEVATION (2 OF 3)

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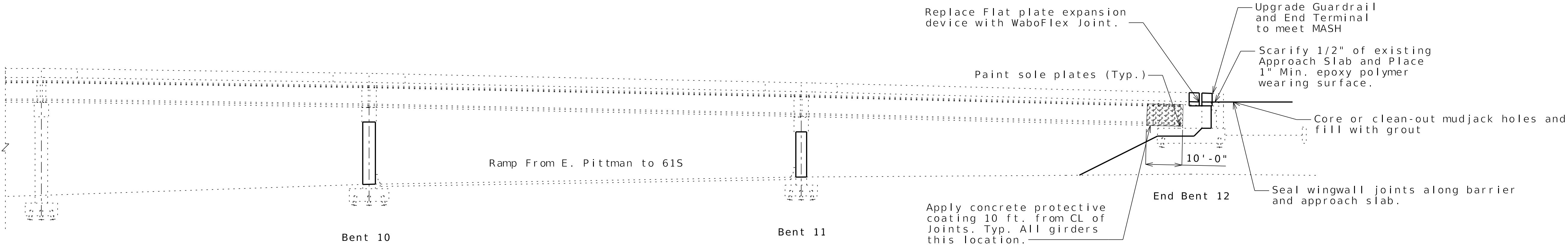
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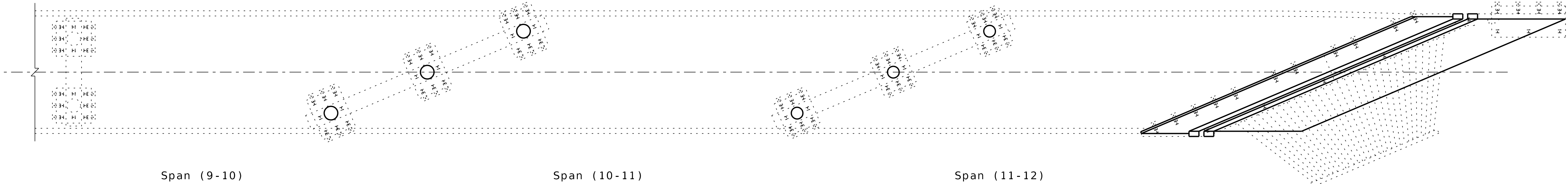
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U.I.P AND REHABILITATE (69' - 69' - 120' - 108' - 104' - 90') (90' - 120' - 91' - 120' - 116') CONTINUOUS PRESTRESSED CONCRETE BULB TEE SPANS



GENERAL ELEVATION



PLAN

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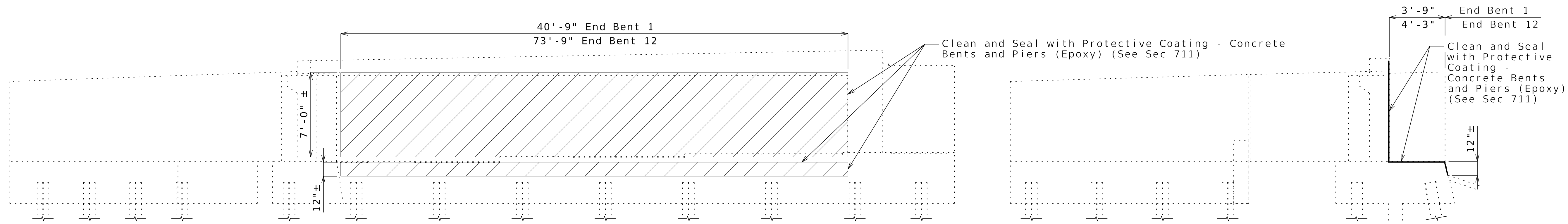
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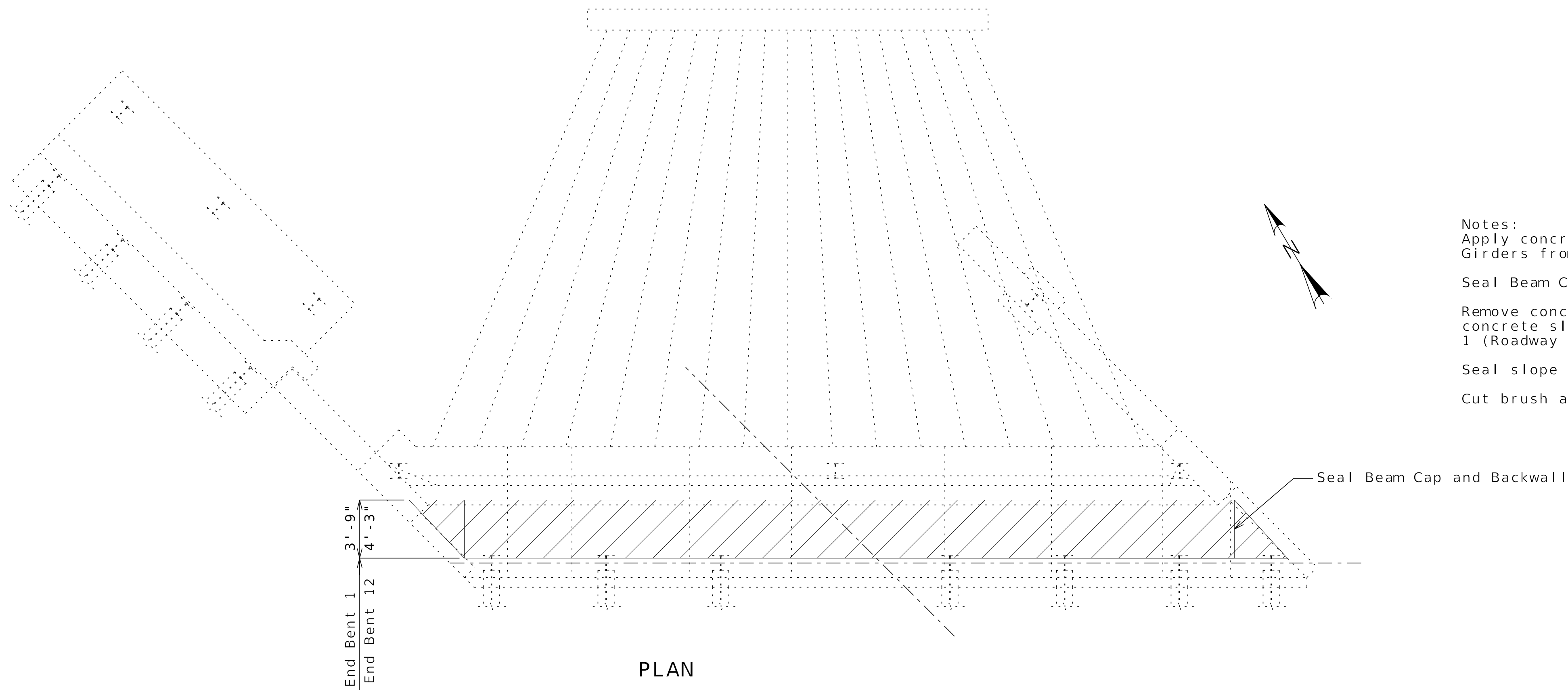
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ELEVATION

END VIEW



PLAN

Notes:
Apply concrete protective coating to 10 Lin. Ft. of P/S Girders from ϕ of joint, See Sheet 8 of 17
Seal Beam Cap and Backwall as shown, (See Sec 711)
Remove concrete slope protection, regrade and place new concrete slope protection (damaged portions only), Bent 1 (Roadway Item)
Seal slope protection joints and cracks (Roadway Item)
Cut brush and trees growing along slopes (Roadway Item)

STATE OF MISSOURI
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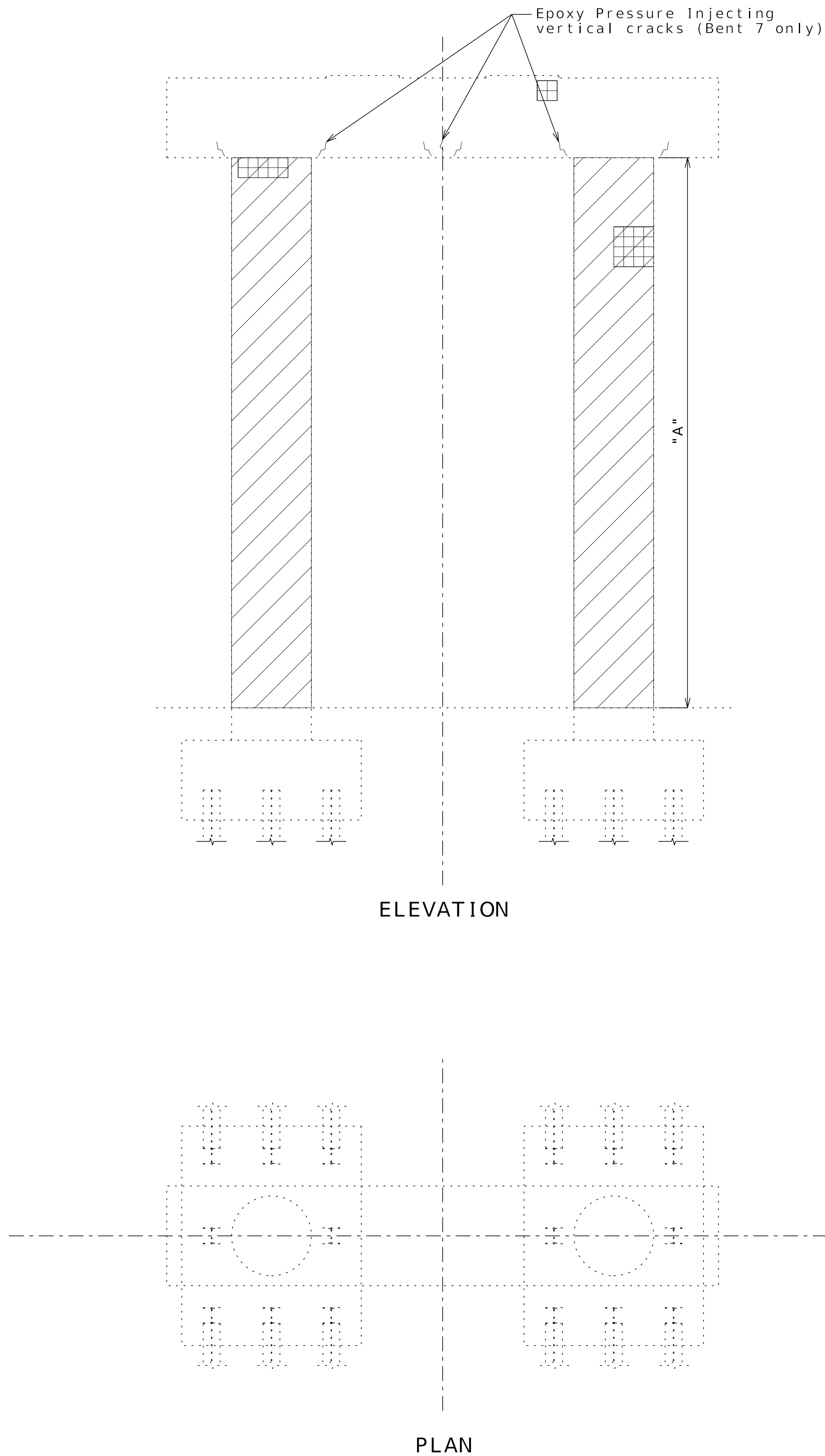
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Note: This drawing is not to scale. Follow dimensions.

Sheet 5 of 20

DETAILS OF END BENT REPAIRS AT END BENTS 1 & 12

B_A54062_005_JST0020_ENDBENTREP.dgn 4:12:54 PM 6/6/2025



Bent	Dim "A"
4	24' - 0" ±
5	26' - 6" ±
6	28' - 0" ±
7	27' - 0" ±

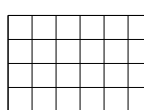
Bent	Dim "B"
4	3' - 6"
5	3' - 0"
6	3' - 0"
7	4' - 0"

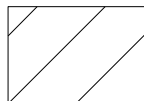
Epoxy Pressure Injection Notes:
The epoxy material shall consist of a two-component system in accordance with the requirements of ASTM C 881, Type IV, Grade 1, except that the viscosity shall be a maximum of 4.5 poise (0.45 Pa*s). The Class designation of the epoxy shall be determined according to the temperature that exists on the job.

The contractor shall furnish manufacturer's certification that the material supplied is in accordance with these specifications. The certification shall include or have attached typical test results for all specified properties required by ASTM C 881 for the injecting resin. The engineer reserves the right to sample and test any or all material supplied.

The surface to receive the epoxy grout shall be cleaned of laitance, grease and foreign matter by sandblasting. The cracks shall be cleaned of debris by using oil-free and water-free compressed air or vacuum. After the cracks are cleaned, the epoxy shall be injected in accordance with manufacturer's recommendations. The temporary surface seal and placement and method of attachment of injection ports shall be in accordance with the epoxy manufacturer's recommendations.

Notes:
Inject Epoxy into cap for Crack Repair
Repair Delaminations in Cap and Column

- 

Repair delaminations (Bent 7 only) with formed substructure repair in accordance with Sec 704. Unformed repairs may be performed at the contractor's option. If unformed repair is used, Shotcrete shall be required in accordance with Shotcrete Concrete Repair JSP.
- 

Clean and Seal with Protective Coating - Concrete Bents and Piers (Epoxy) (See Sec 711)

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COUNTY
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JOB NO.
JST0020

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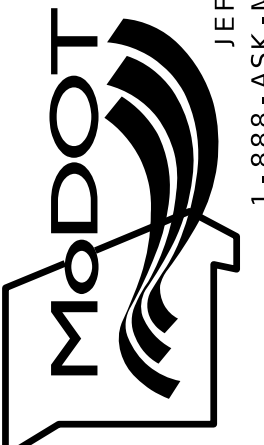
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BRIDGE NO.
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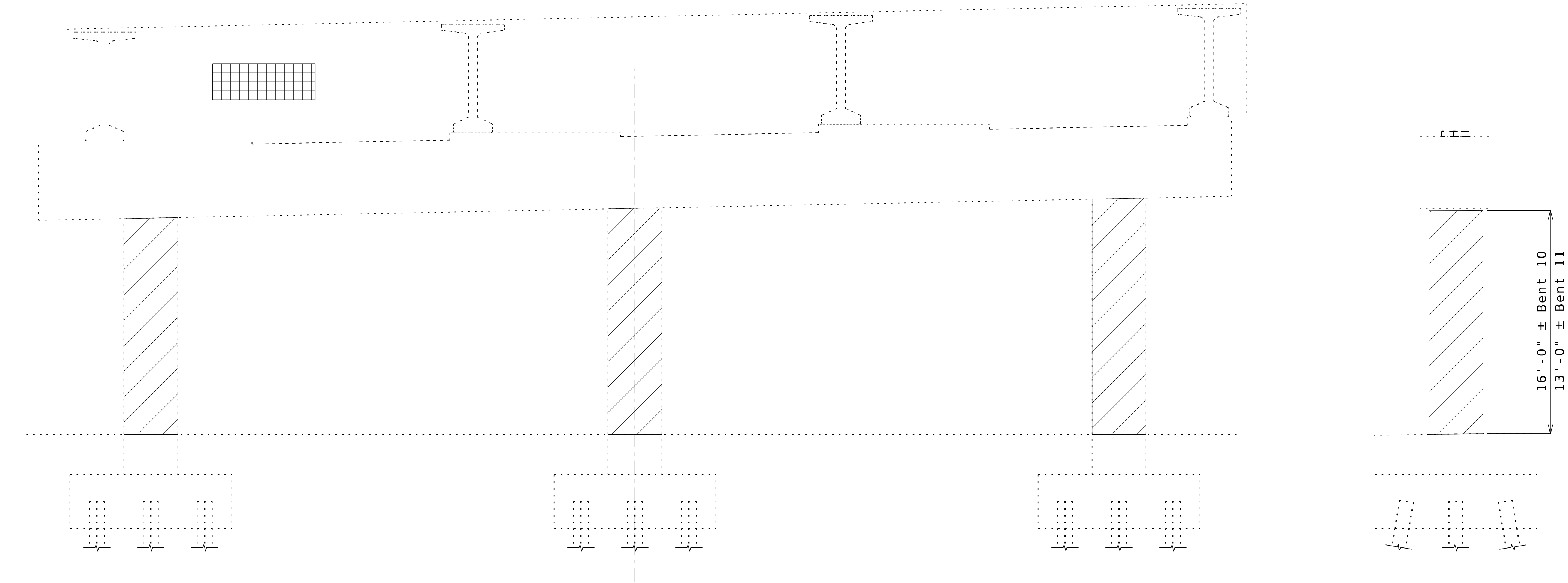
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Sheet 6 of 20

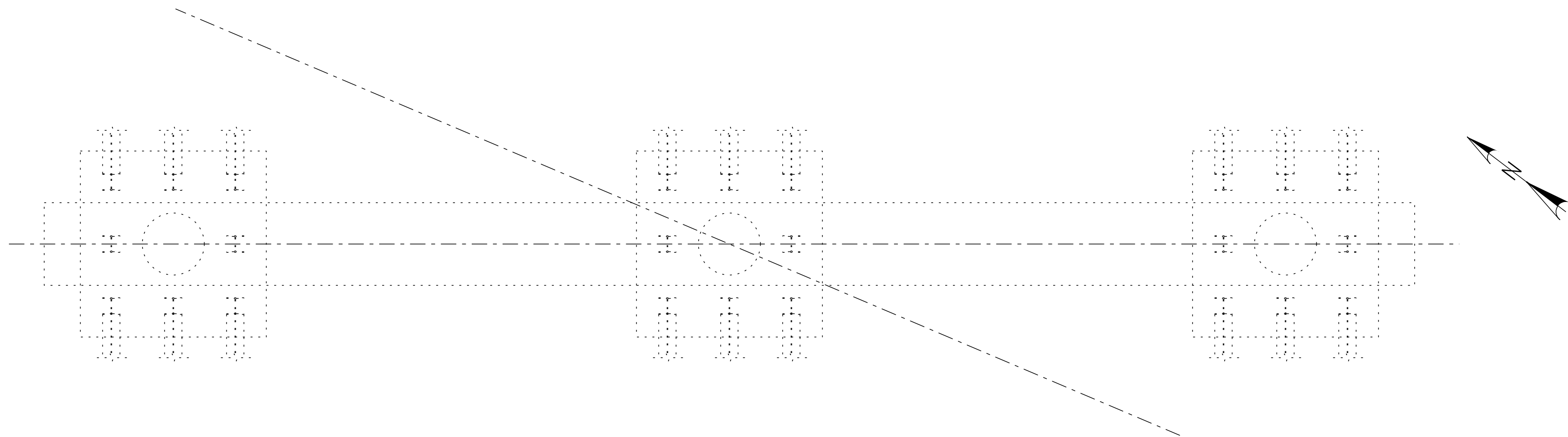
DETAILS OF REPAIRS AT BENTS 4 THRU 7

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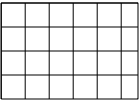
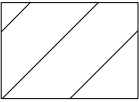


ELEVATION

END VIEW



PLAN

- Notes:
-  Clean and apply protective coating to spalled surfaces of concrete diaphragm, Bent 11 only. (See Special Provisions)
 -  Clean and Seal with Protective Coating - Concrete Bents and Piers (Epoxy) (See Sec 711)

STATE OF MISSOURI
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ROUTE
1 - 70

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BR A54062-8

COUNTY

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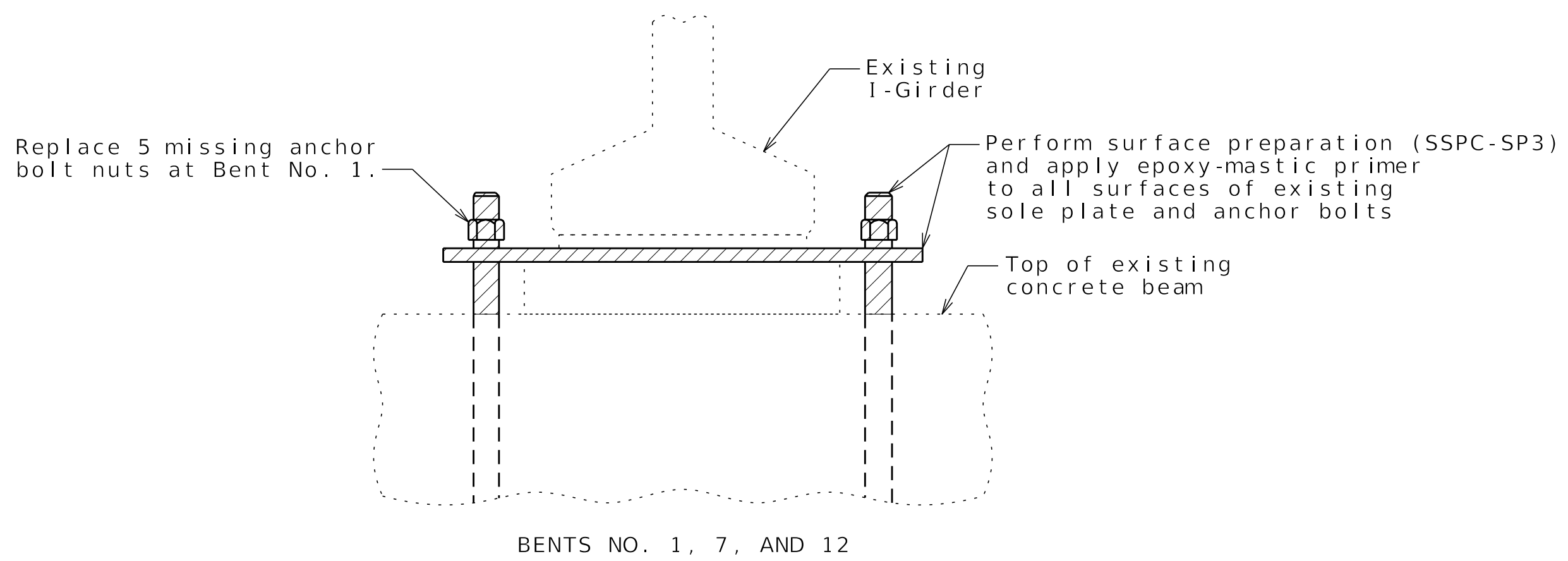
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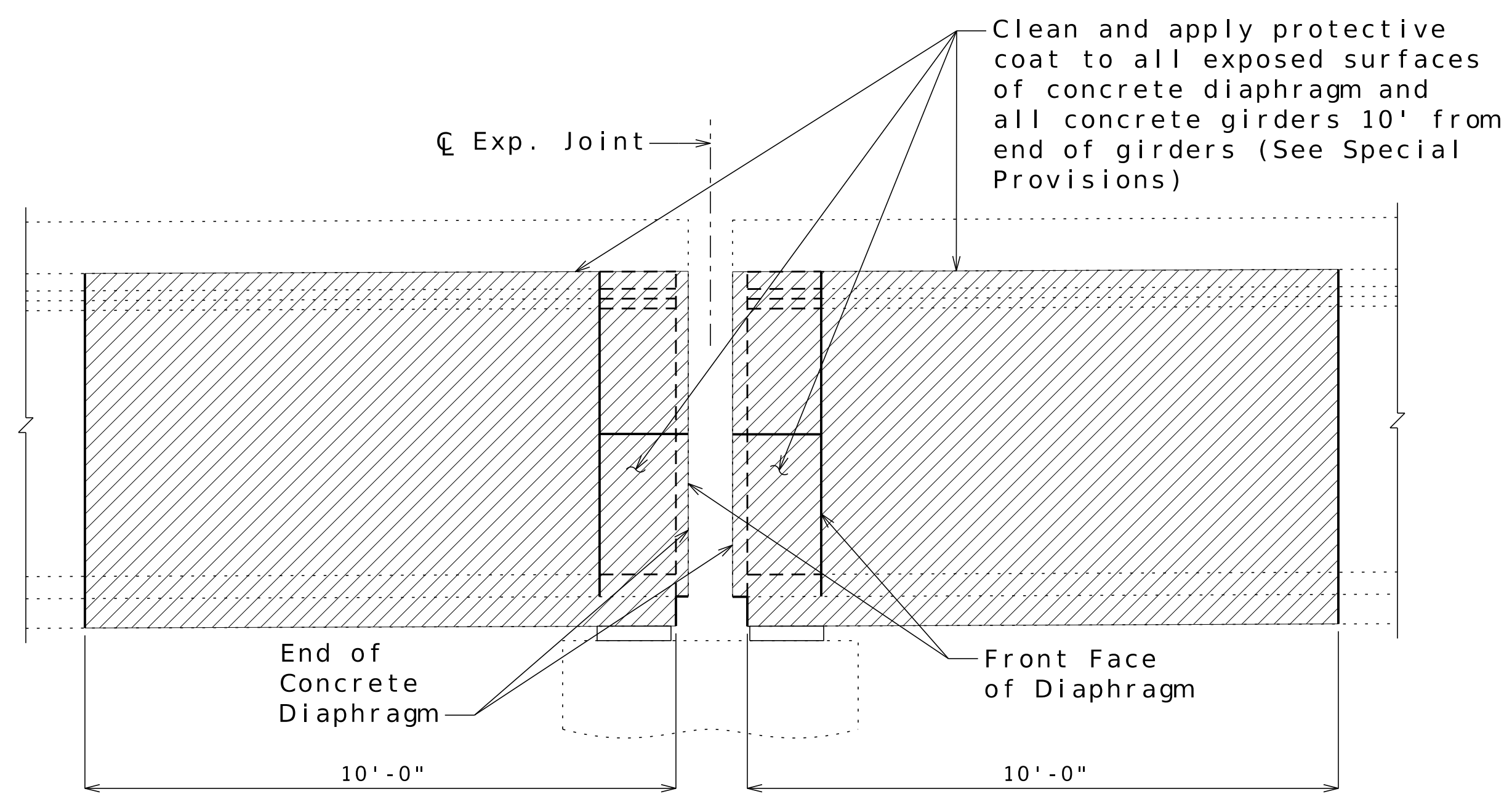
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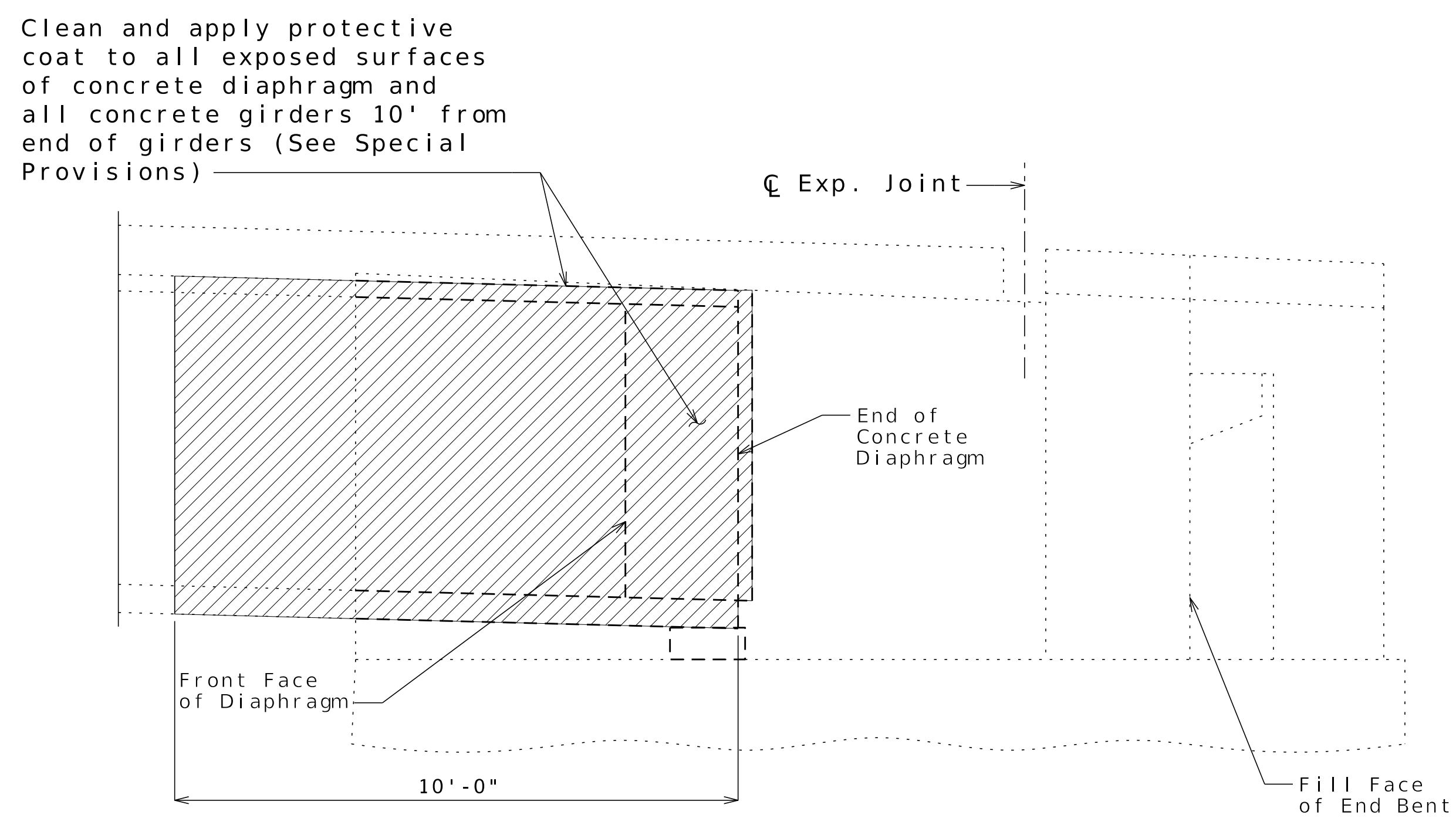
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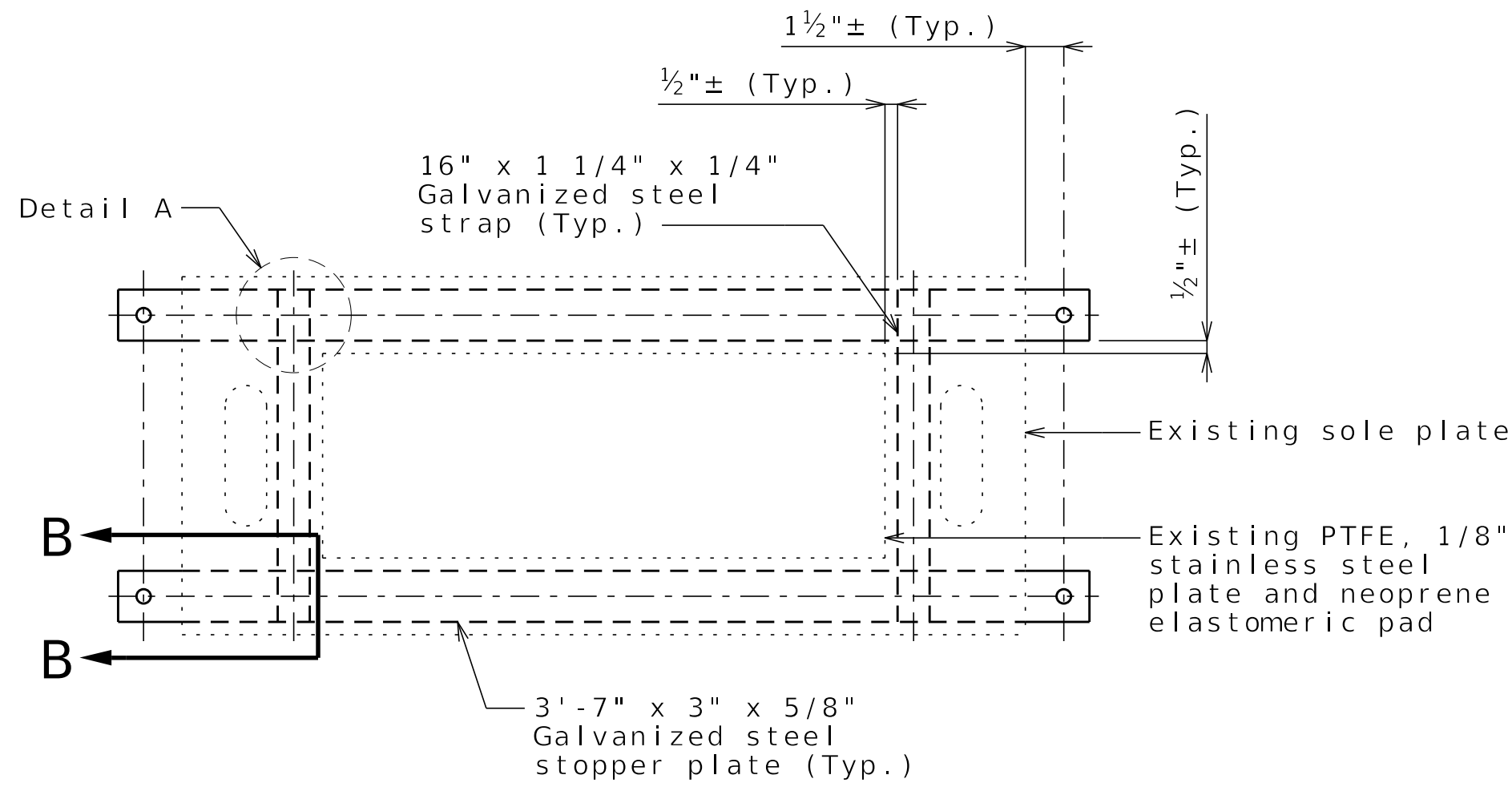
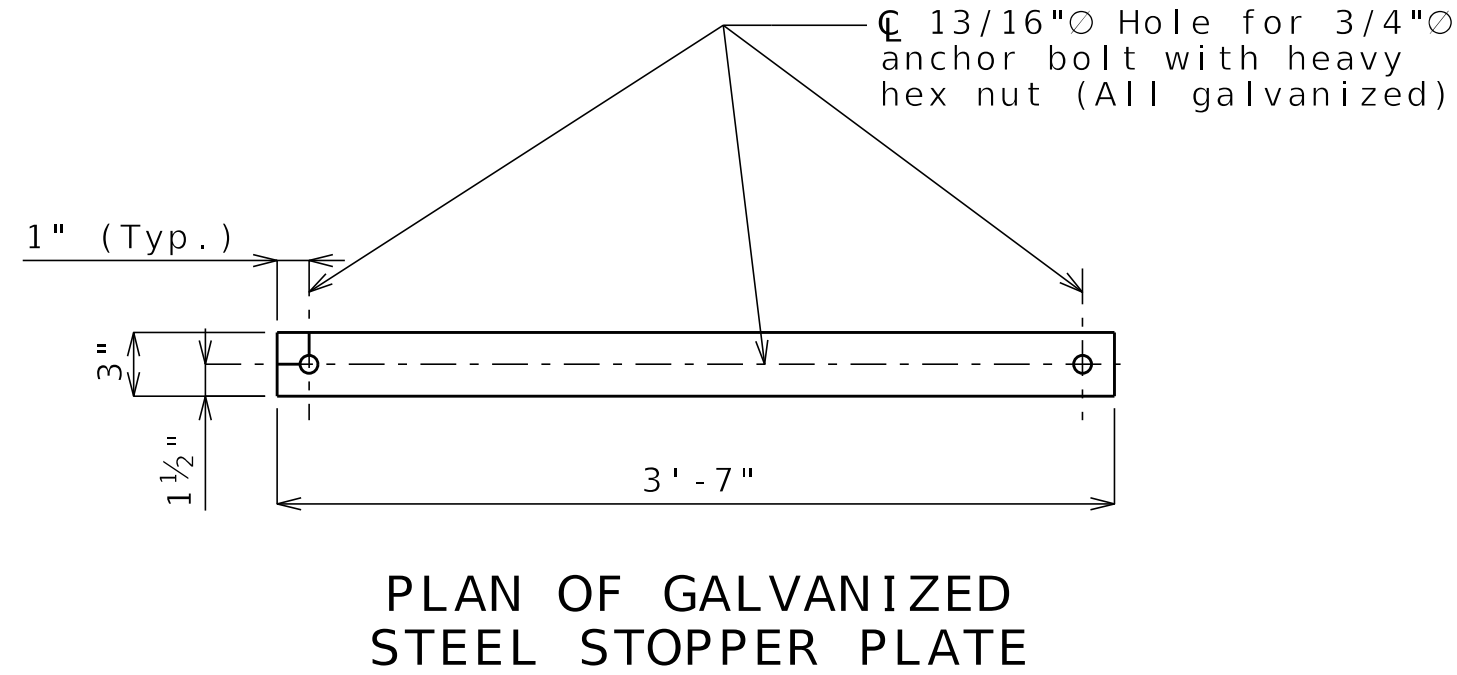
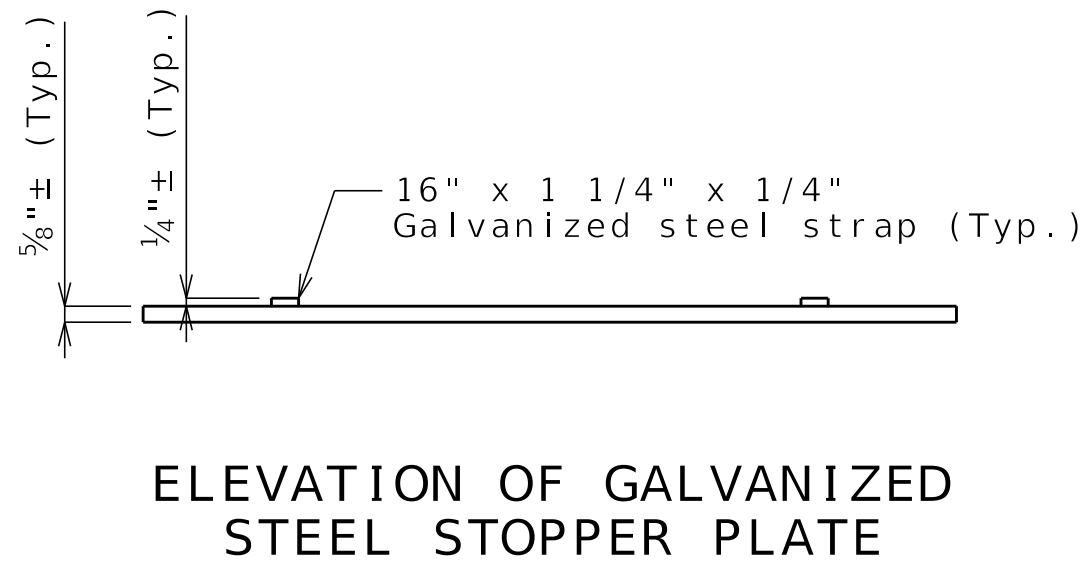
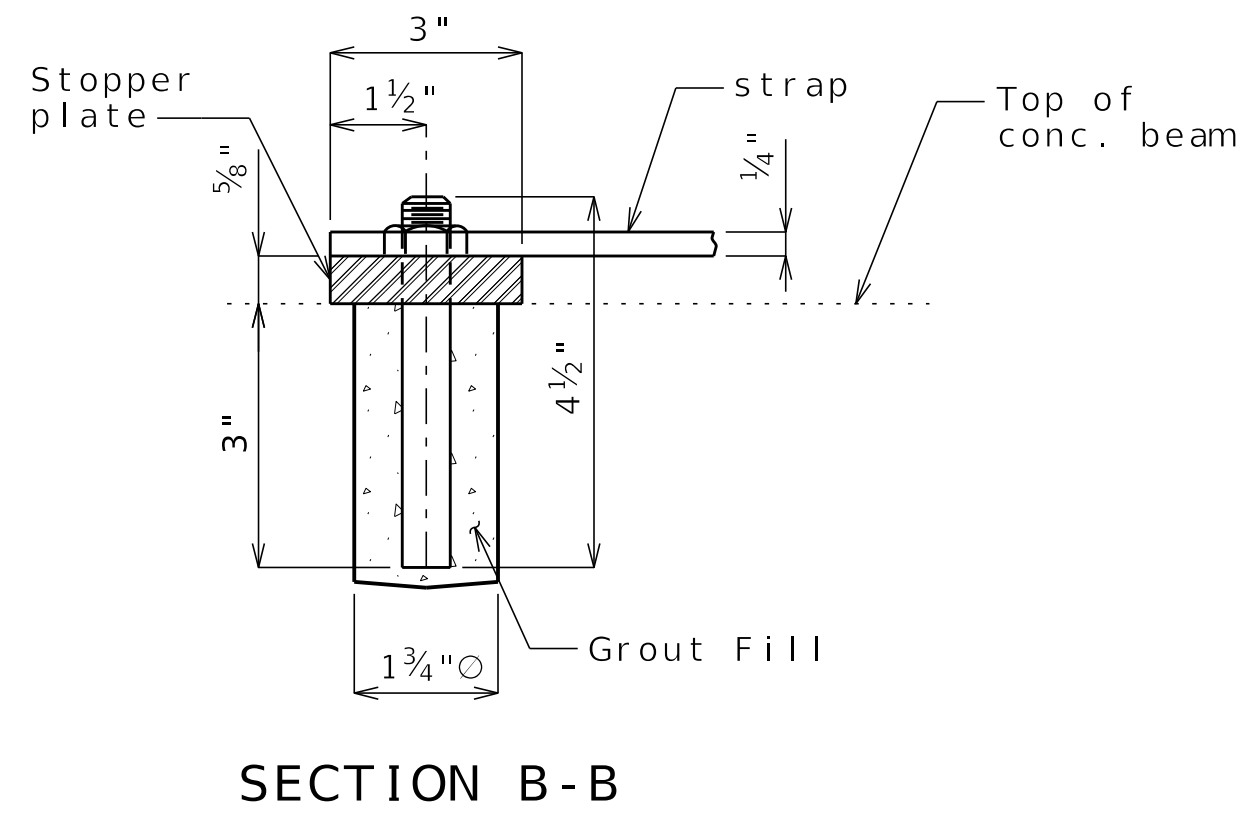
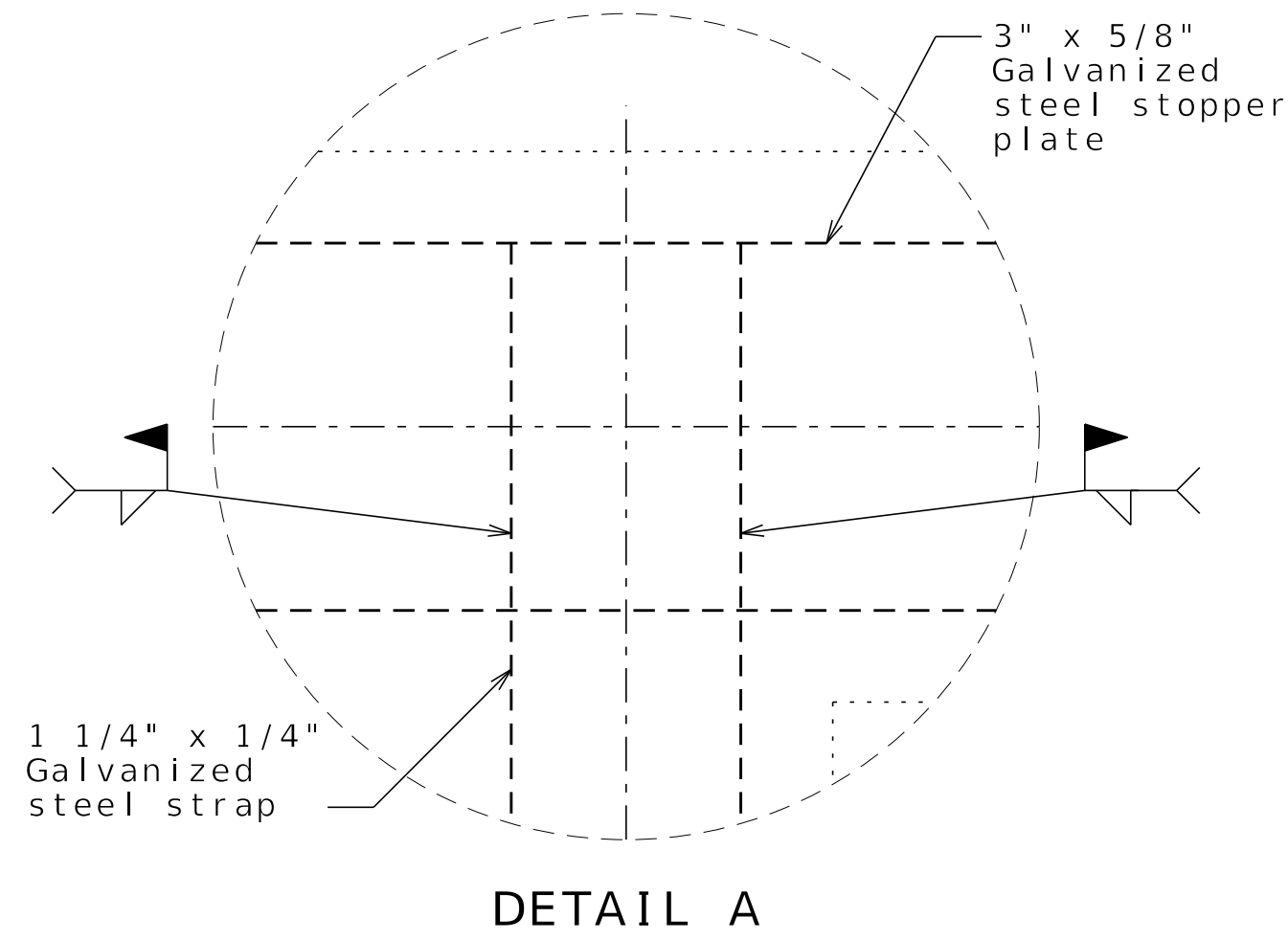
TYPICAL SECTION OF BEARING ASSEMBLY
SHOWING EPOXY-MASTIC OVERCOAT OF
SOLE PLATES AND ANCHOR BOLTS
(Typical at 16 Bearing Locations)



DETAIL SHOWING LIMITS OF
CLEANING AND PROTECTIVE COATING
INT. BENT NO. 7



DETAIL SHOWING LIMITS OF
CLEANING AND PROTECTIVE COATING
END BENTS NO. 1 AND 12
(End Bent No. 12 shown, End Bent No. 1 similar)



(Existing girder not shown for clarity)
(No. Assemblies Required: 8)

Notes:

Structural steel for the stopper plates and straps shall be ASTM A709 Grade 50 and shall be galvanized in accordance with Sec 1081.

Stopper plates and straps shall be provided to prevent loss of support due to creeping of bearings.

Shift new anchor bolt well the minimum extent necessary to avoid damaging the existing beam reinforcing.

Anchor bolts and heavy hex nuts shall be galvanized in accordance with Sec 1081.

Galvanizing material shall be omitted or removed one inch clear of field weld locations. The method used to omit or remove the galvanizing material shall be masking, grinding or other methods as approved by the engineer. Field galvanize the field weld area in accordance with Sec 1081 by zinc alloy stick method.

Remove existing stopper plates and cut off existing anchor bolts 1" below top of beam and fill with an approved qualified special mortar.

DETAILS OF STOPPER PLATES

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1 - 70

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DESCRIPTION	DATE

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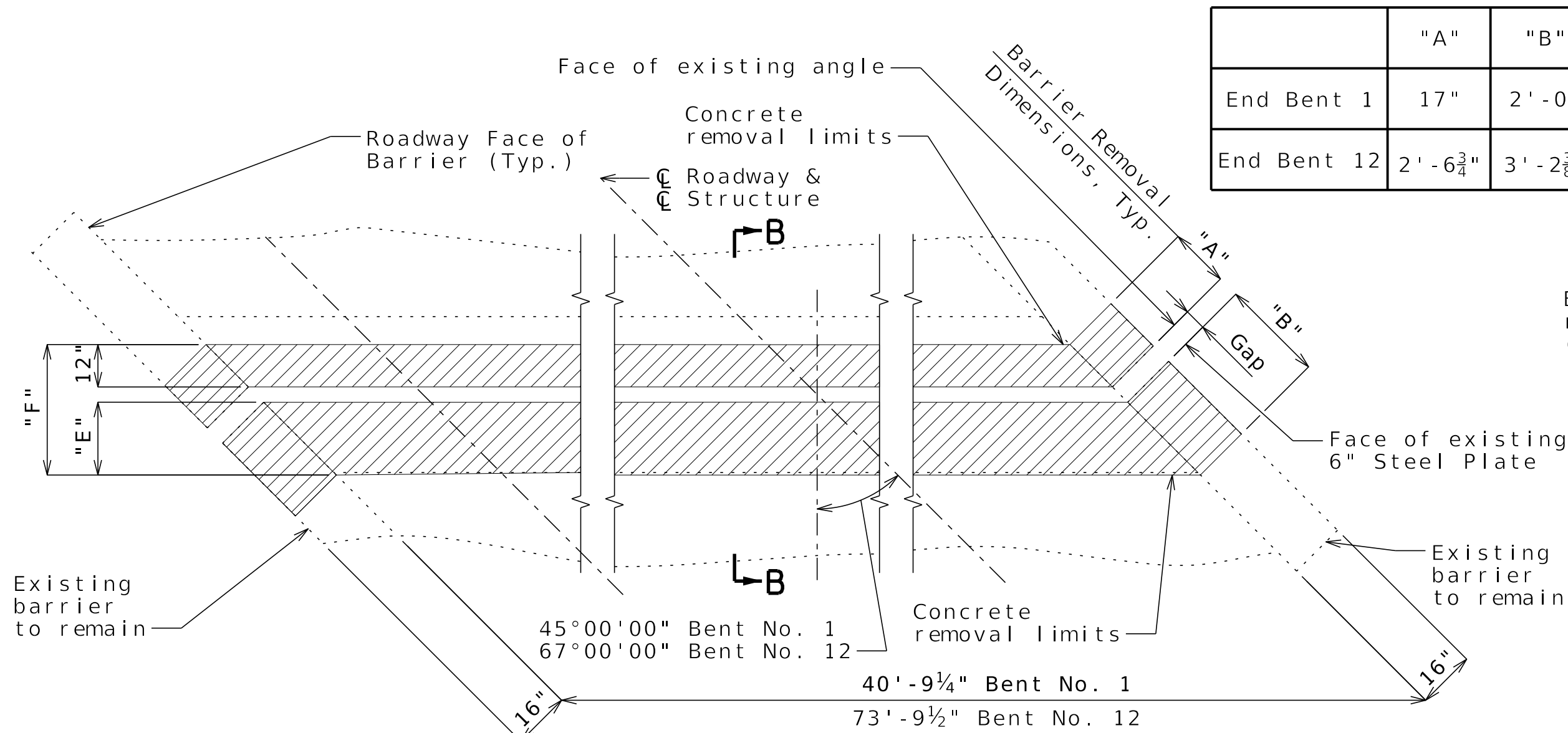
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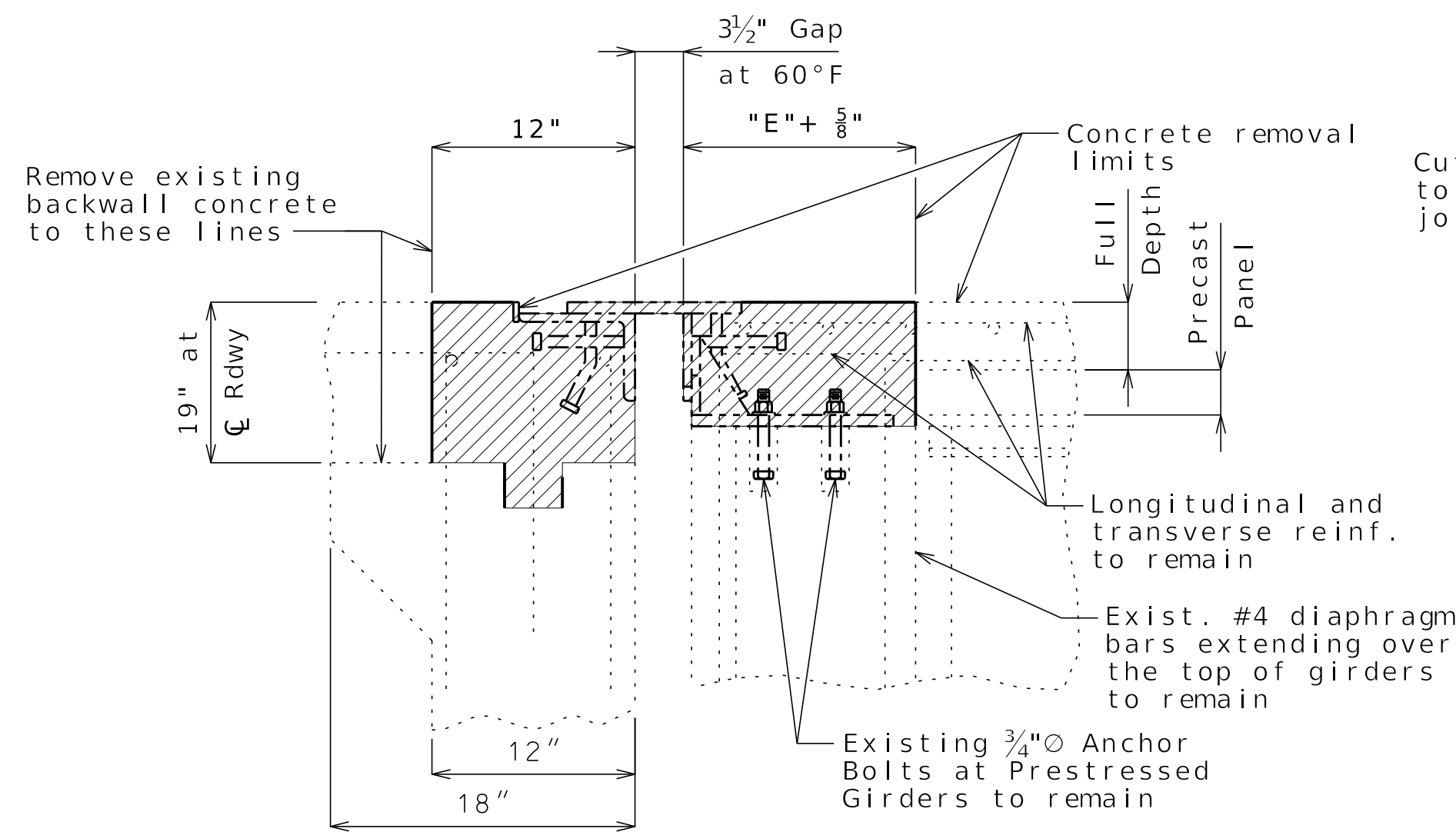
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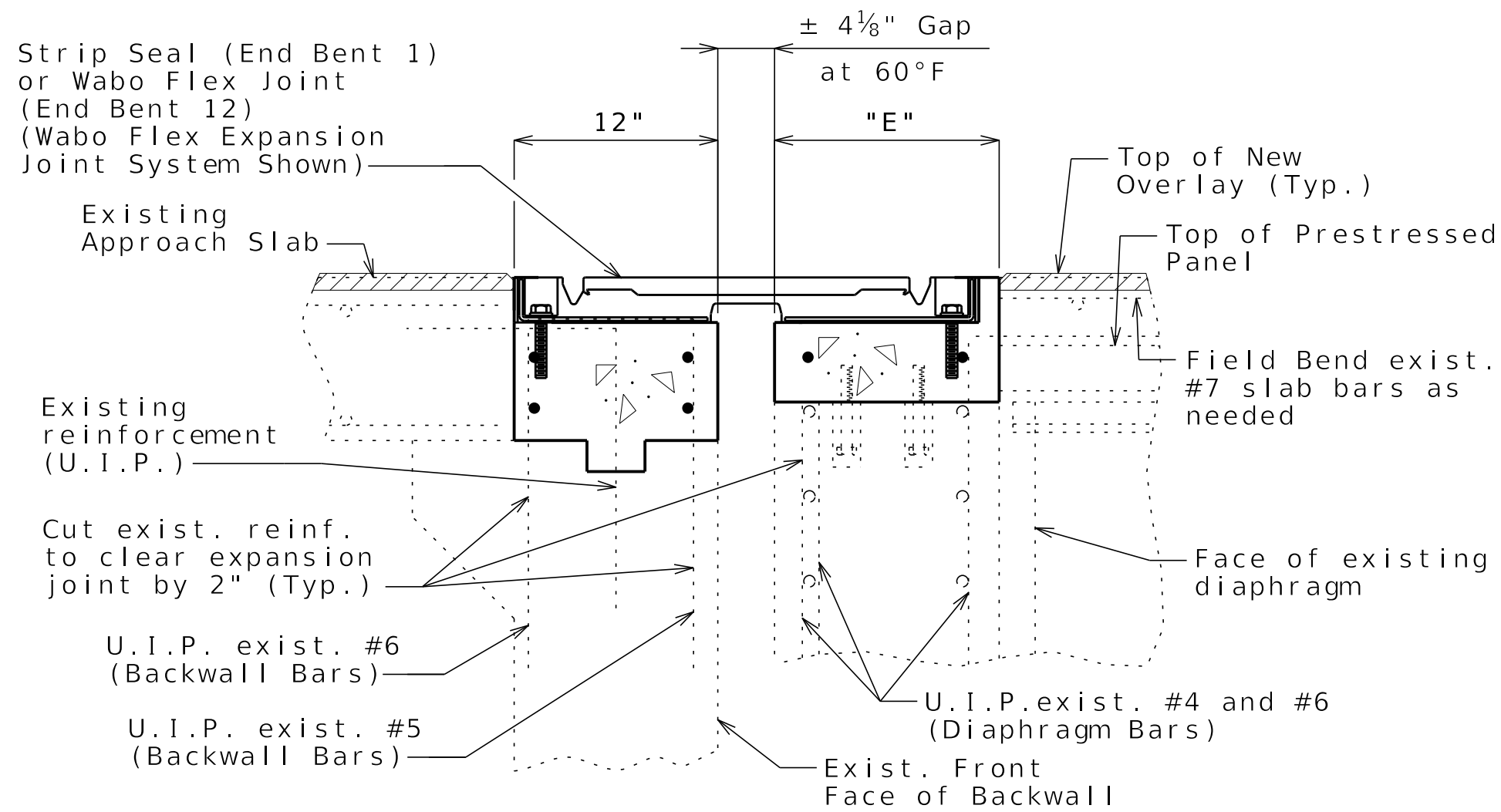
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REMOVAL PLAN AT INTERMEDIATE END BENT NO. 1
End Bent No. 12 Similar and Opposite Hand



SECTION B-B



SLAB REPLACEMENT DETAIL
Note: Full depth slab replacement required over Girder and overhangs.

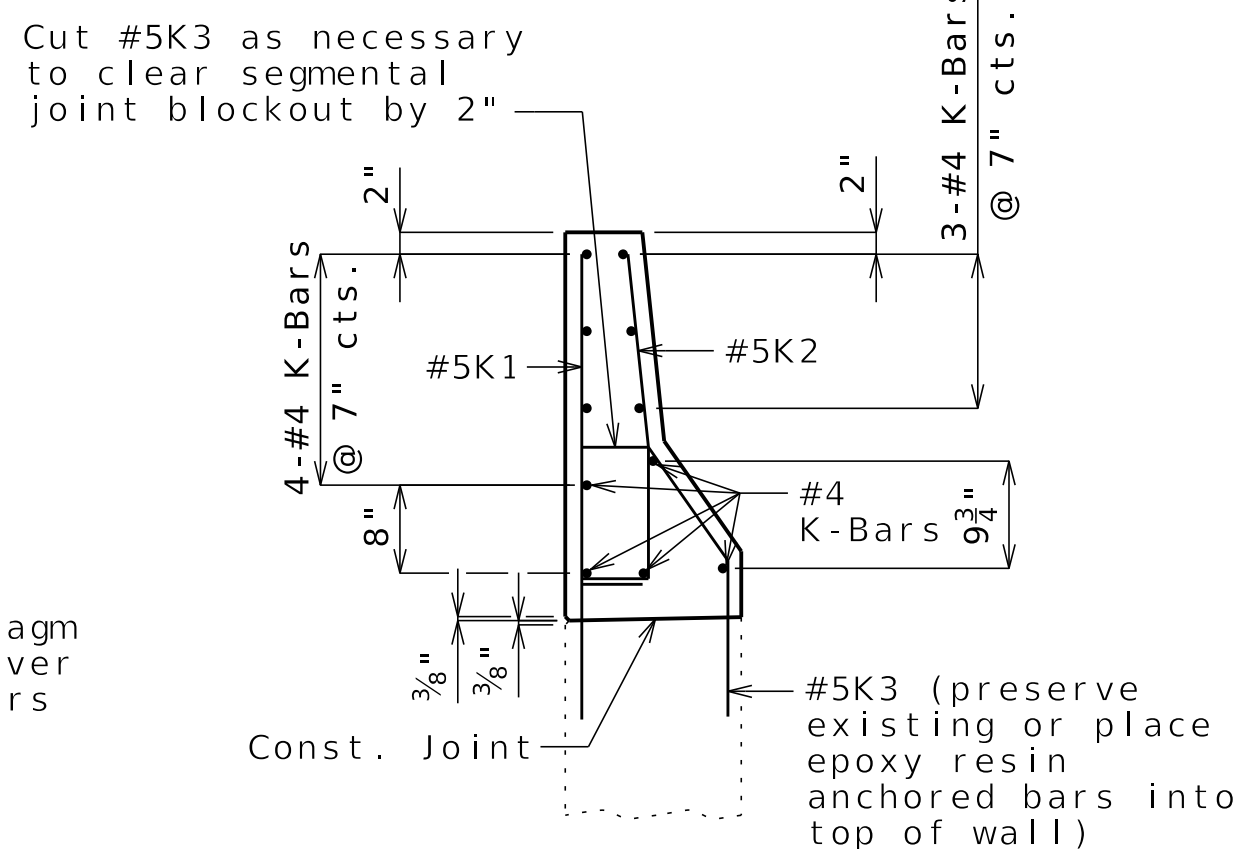
	"A"	"B"	"C"	"D"	"E"	"F"
End Bent 1	17"	2'-0"	3	3	17"	2'-8 1/2"
End Bent 12	2'-6 3/4"	3'-2 3/8"	5	4	15"	2'-6 1/2"

Existing longitudinal reinforcement (U.I.P.) (Typ.)

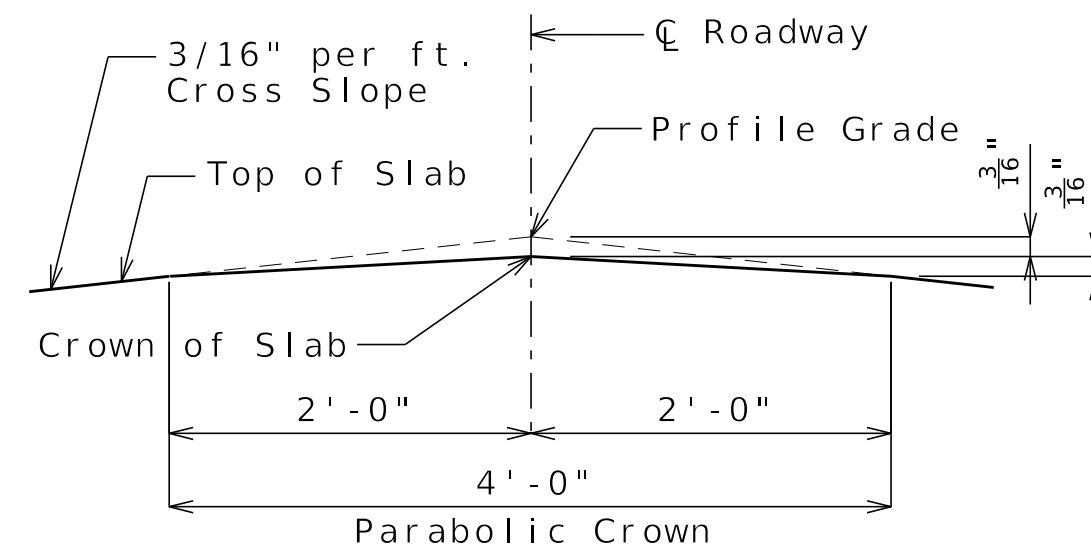
#6-H1 Bars(End Bent 1)
#6-H12 Bars(End Bent 12)
(Lap #6-H12 a min. of 3'-7" at center of bridge)

"C"-#5-K1, K2
with epoxy resin anchored
#5-K3 bars (Eq. Spa.)

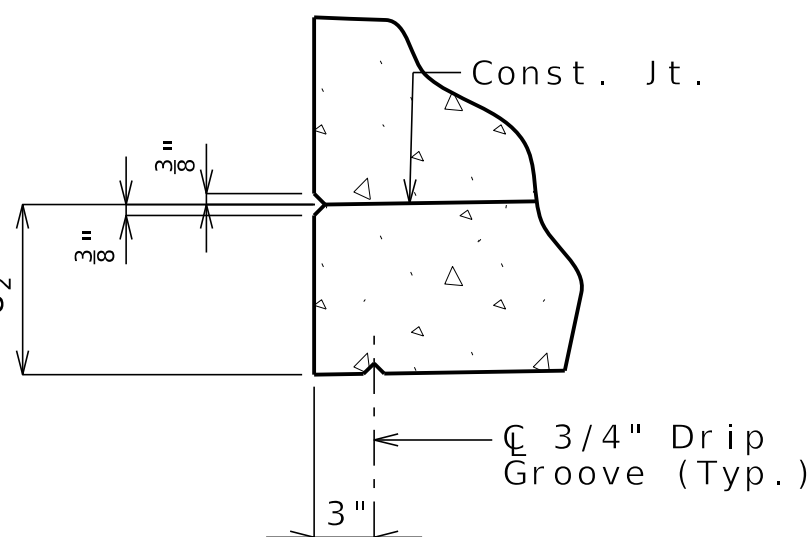
PART ELEVATION OF BARRIER



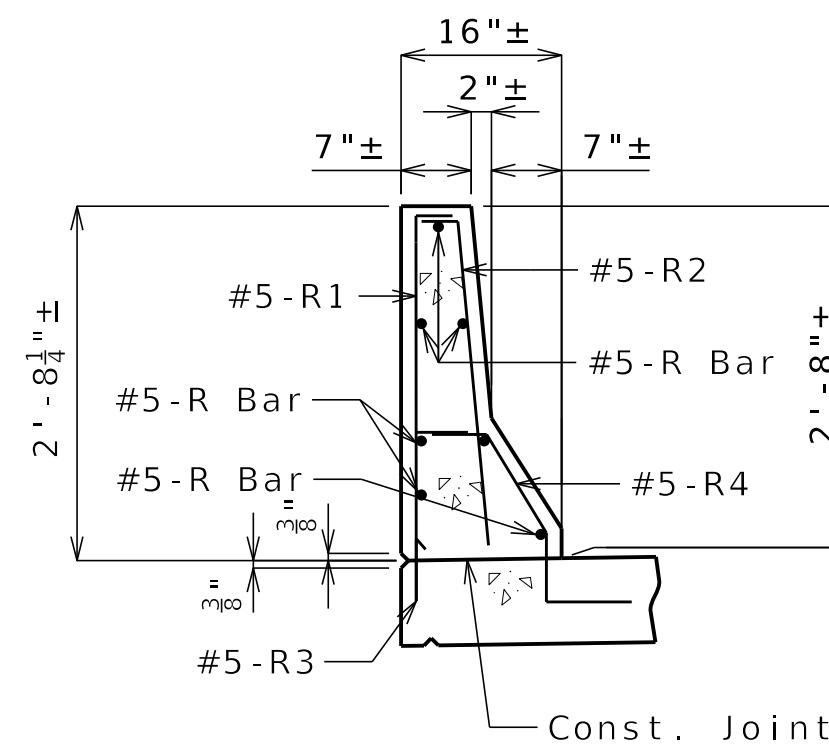
SECTION C-C



CROWN DETAIL AT END BENT NO. 1
SUPERELEVATION AT END BENT NO. 12



DRIP GROOVE DETAIL



SECTION D-D

The cross-sectional area above the slab is 2.27 square feet.

Cut or bend existing longitudinal #5-R bars & #5-R4 to clear segmental joint blockout by 2" as needed.

At the contractors option the R1 and R2 Bars can be substituted for a single bar. See Sheet 12 for details.

Notes:

Sections shown are taken at C Structure between girders. Contractor shall preserve girder reinforcing that extends into new concrete if not in conflict with the new joint placement.

Remove concrete in accordance with Sec 216.

Removal shall include removal of the existing expansion joint armor and anchorage as detailed.

Concrete removal to provide 1" minimum clearance beneath exposed reinforcing to remain. Removal Depth may be increased as needed to accommodate new anchorage requirements for the replacement joint as per the manufacturer's recommendations.

Any existing concrete traffic delineators on the barrier curb removed during the joint replacement process shall be replaced with like and kind.

Barrier curb to be built parallel to grade with barrier curb joints on superstructure. Barrier Curb on end bent shall be built normal to grade.

Top of WaboFlex Joint shall be recessed 1/4" from the top of finished overlay.

Lap Splice lengths where required shall be: 2'-5" for #4 Bars and 3'-7" for #6 Bars.

All exposed edges of barrier curb (Type B) shall match existing barrier curb edges.

Existing Reinforcing may be replaced with like and in kind in accordance with Sec 216.

The contractor may opt to preserve existing reinforcing in-place in lieu of installing epoxy resin anchors.

Any damage to the Existing Reinforcing Epoxy Coating shall be repaired according to Sec 710.

Concrete work shall be in accordance with Sec 704.

The minimum embedment depth in concrete with $f'c = 4,000$ psi for the resin anchor systems shall be that required to meet the minimum ultimate pullout strength in accordance with Sec 1039 but shall not be less than 5". Lengths provided in bar list use 9 times the bar diameter. Contractor shall adjust based on manufacturer's recommendation.

The contractor shall use one of the qualified resin anchor systems in accordance with Sec 1039.

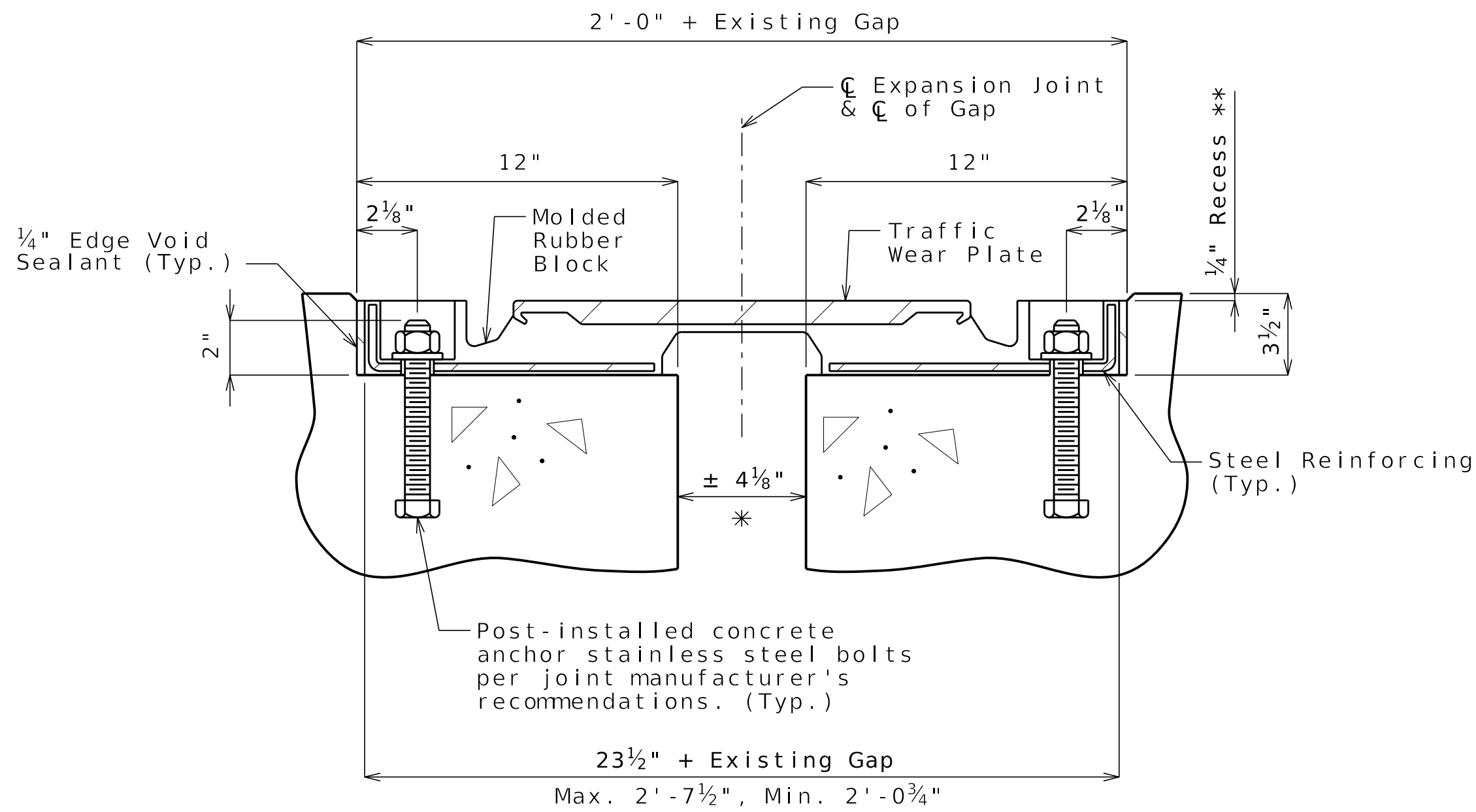
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David J. Glasstetter
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1-70
DISTRICT
BR
COUNTY
ST. CHARLES
JOB NO.
JST0020
CONTRACT ID.
PROJECT NO.
BRIDGE NO.
A54062

DESCRIPTION
DATE

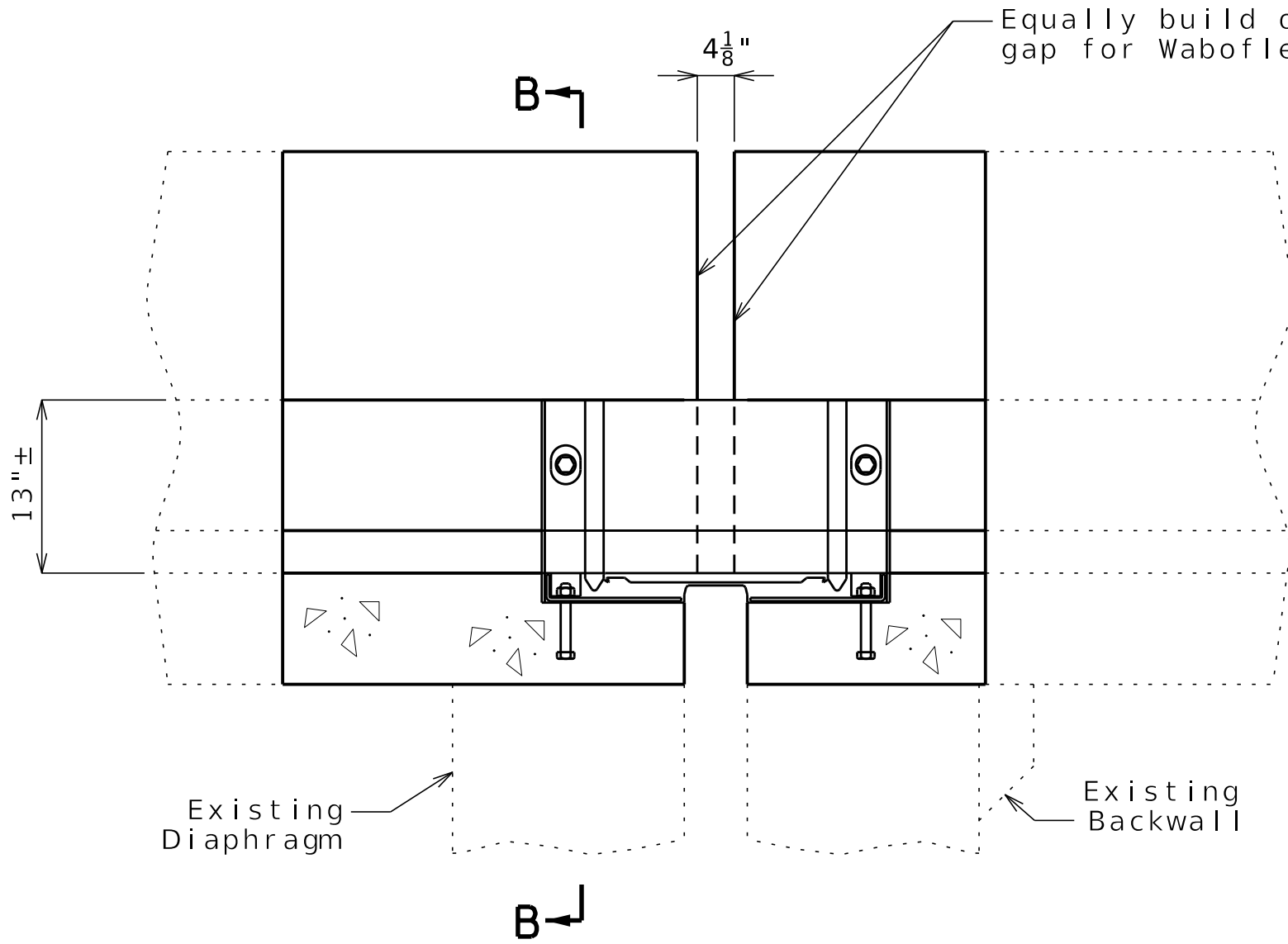
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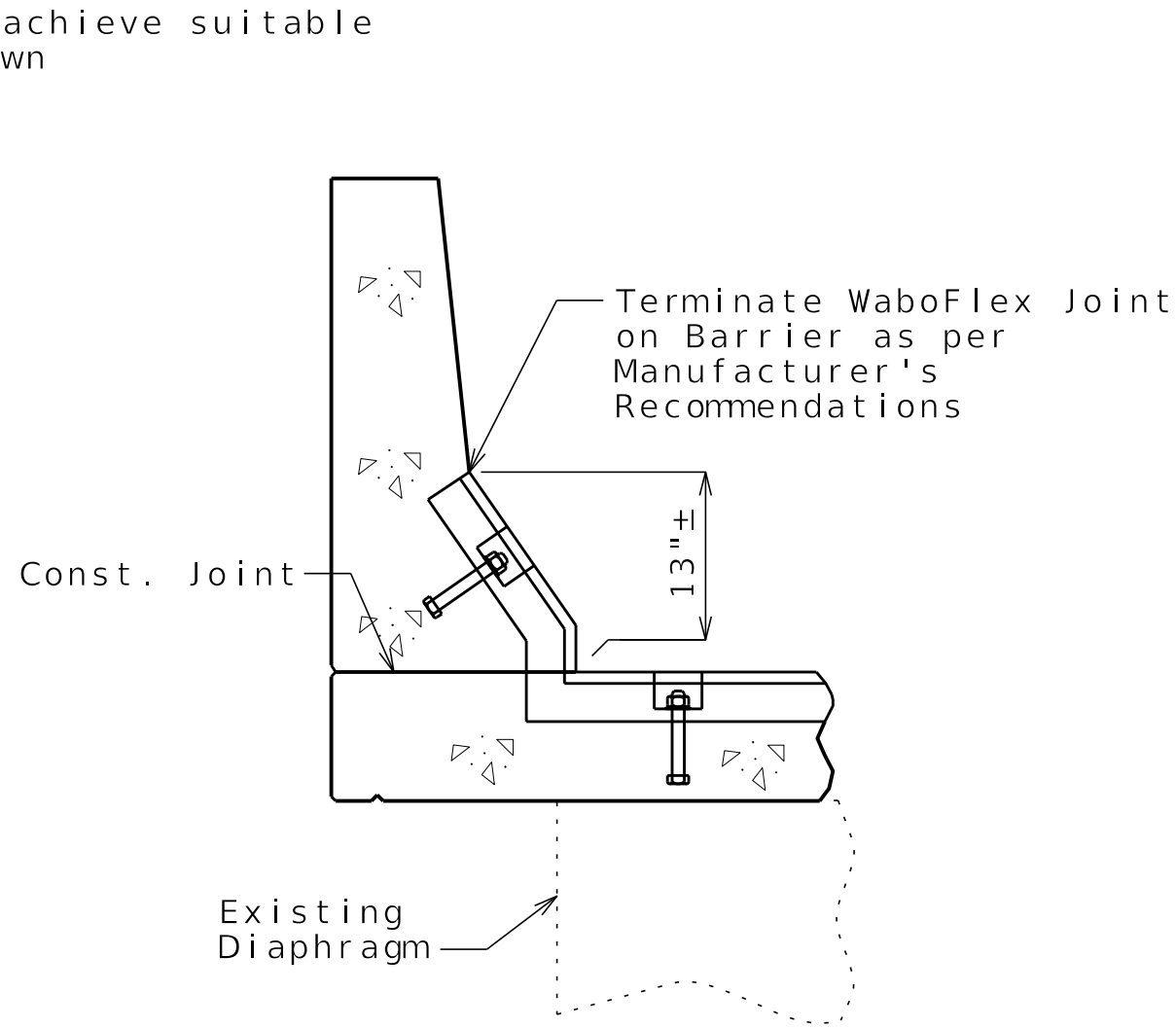
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PHONE 874-3331
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Bartlett & West
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SECTION THRU JOINT
(Use SR 6.5A at End Bent 12 unless otherwise noted by the manufacturer)
(For new reinforcing not shown, See Sheet No. 10)



PART ELEVATION OF BARRIER



SECTION B-B

* Existing plans provided 3 1/2" gap from face of 8"x6"x5/8" angle to mounted 6"x5/8" plate on superstructure at 60°F at time of construction. For replacement joint, adjust gap dimension as needed to match backwall and diaphragm faces.

Gap dimensions are based on installation at 60°F. The expansion gap and other dimensions shall be increased 1/8" for each 10° fall and decreased 1/8" for each 10° rise in temperature at installation.

Notes:

The installation temperature shall be taken as the actual air temperature averaged over the 24-hour period immediately preceding installation.

Replacement expansion joint shall be a WaboFlex Model SR6.5A as manufactured by Watson Bowman Acme Corp. Amherst New York. The new joint is to be installed per manufacturer's recommendations.

The expansion device shall be fabricated to match the crown of the roadway.

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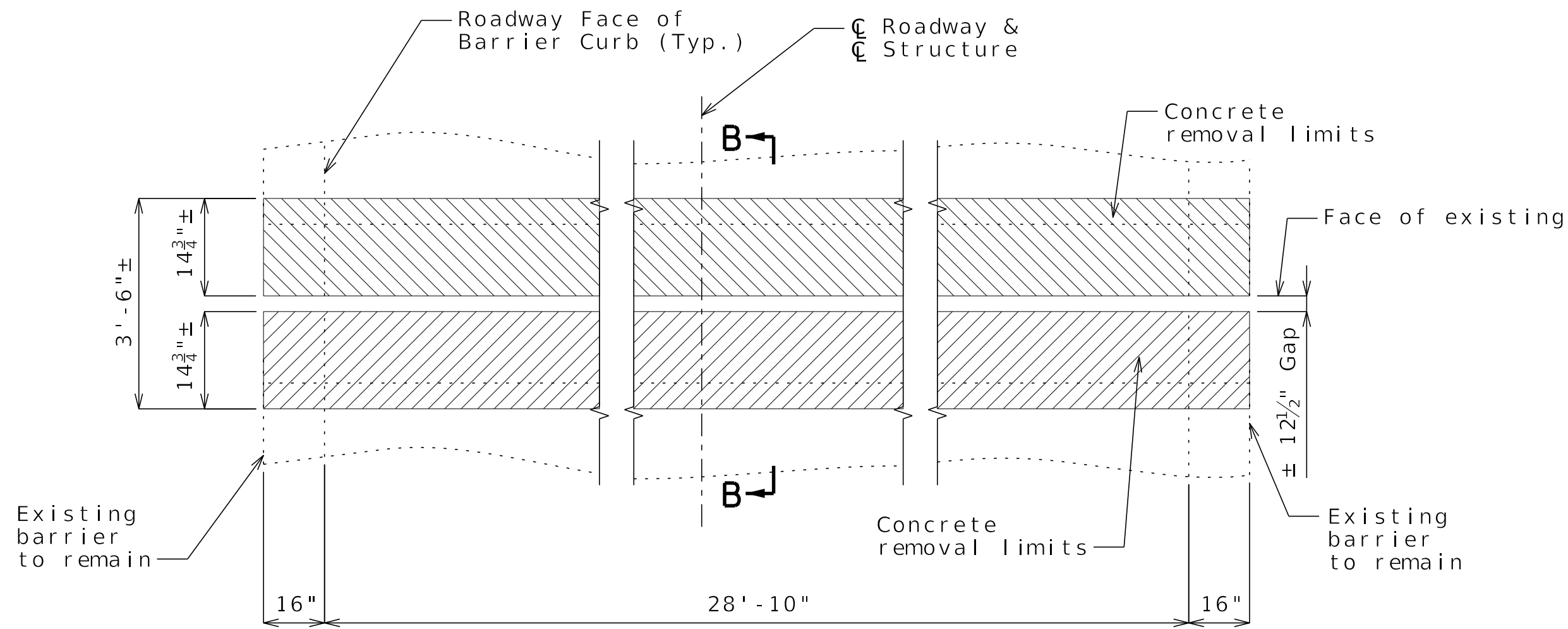
Detailed JUN 2025
Checked JUN 2025

Note: This drawing is not to scale. Follow dimensions.

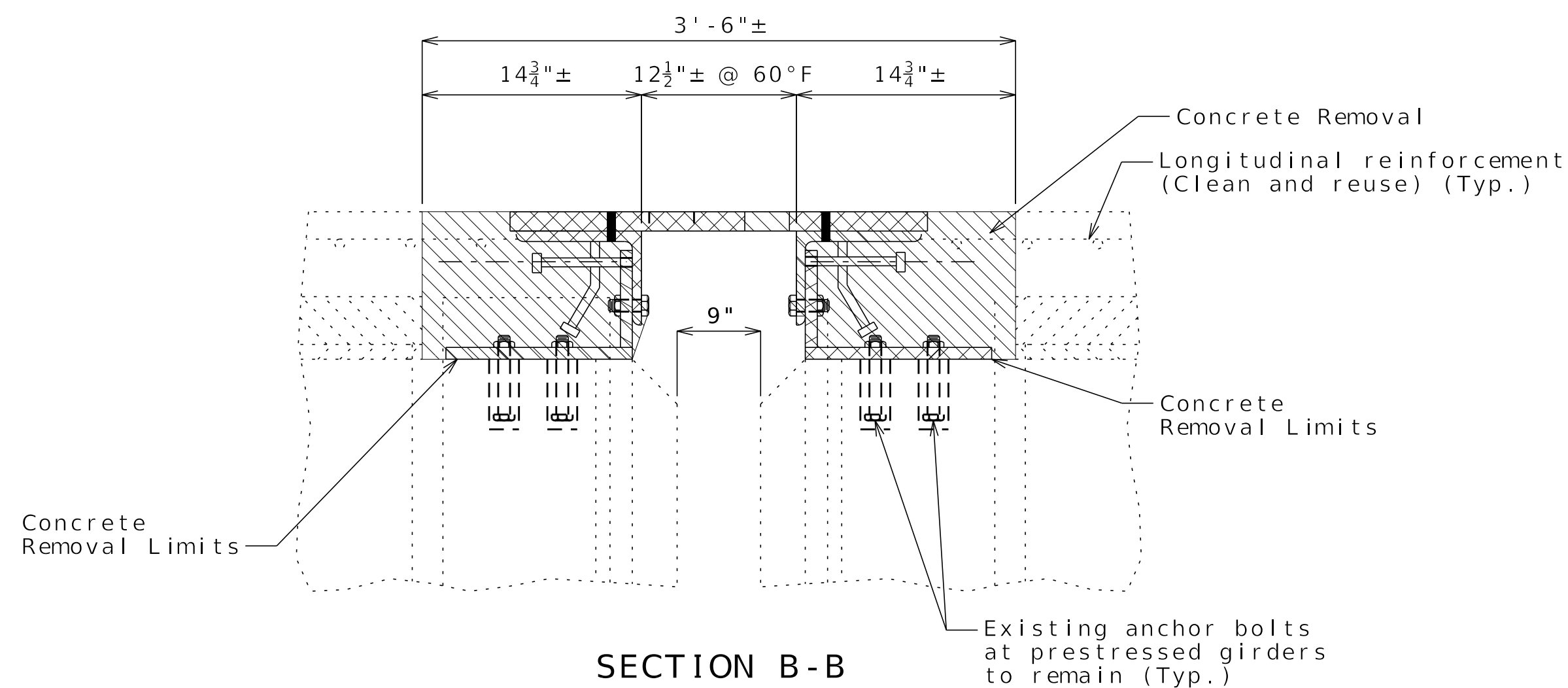
Sheet 12 of 20

SEGMENTAL EXPANSION JOINT SYSTEM AT END BENT NO. 12

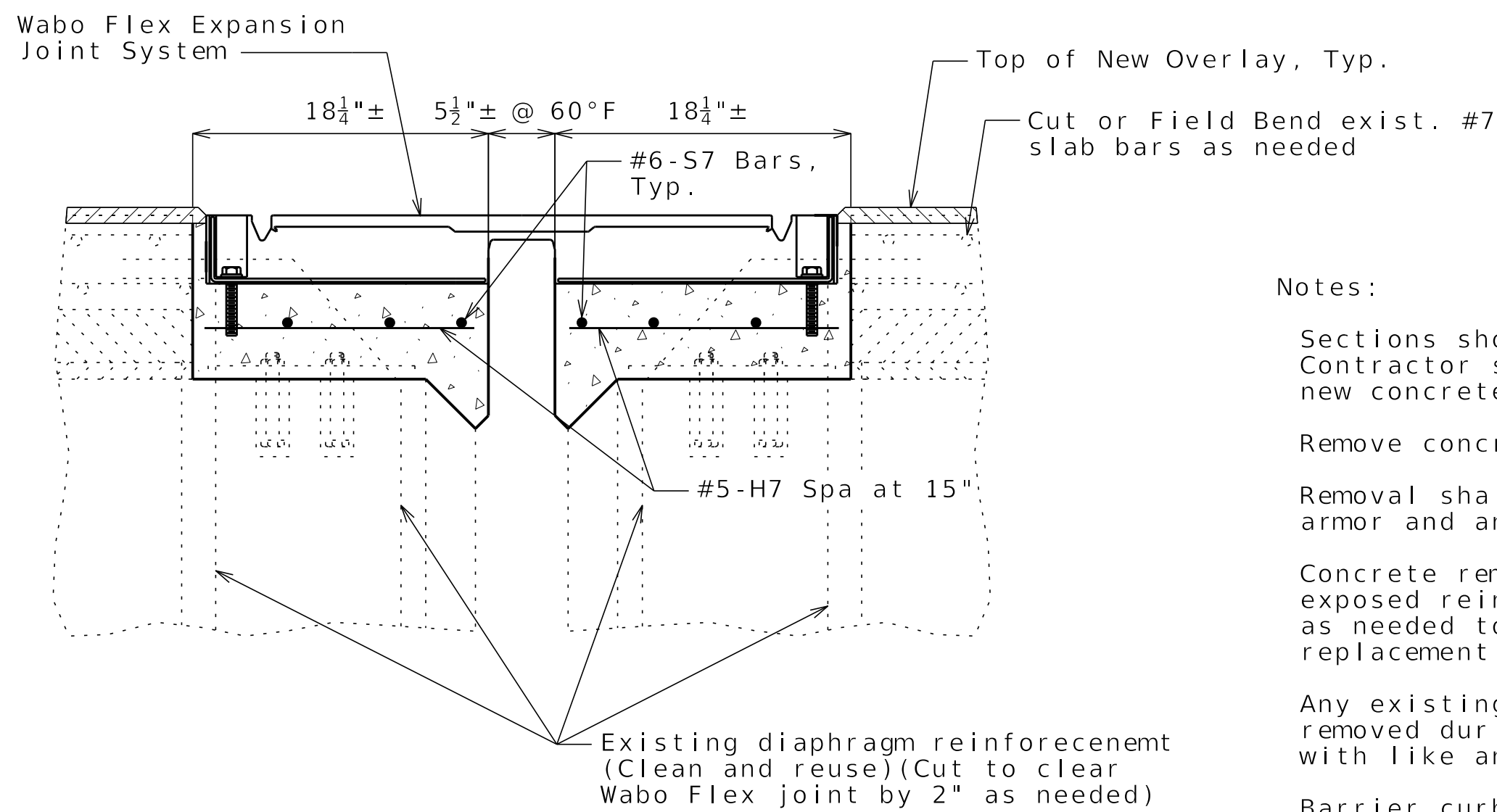
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REMOVAL PLAN AT INTERMEDIATE BENT NO. 7

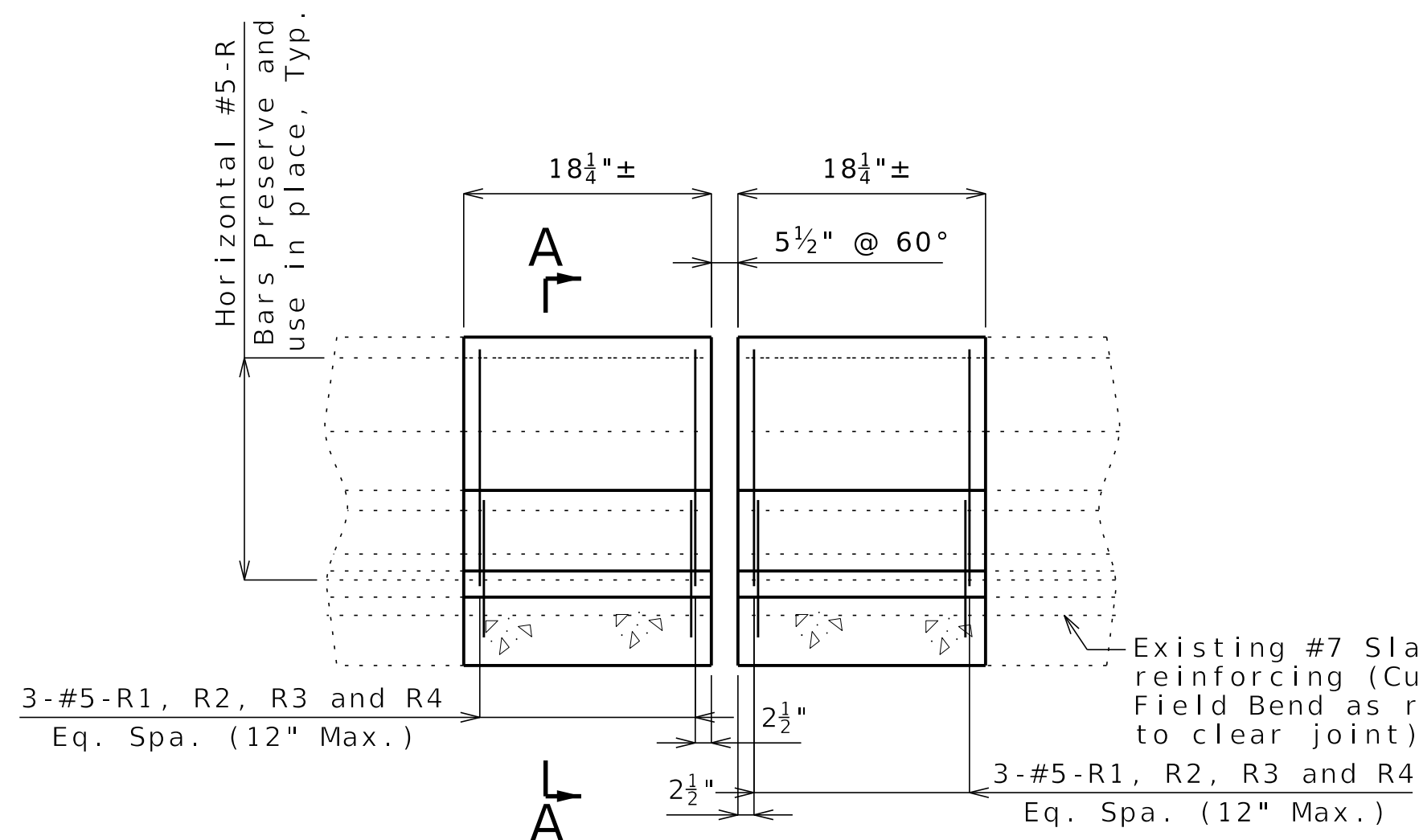


SECTION B-B

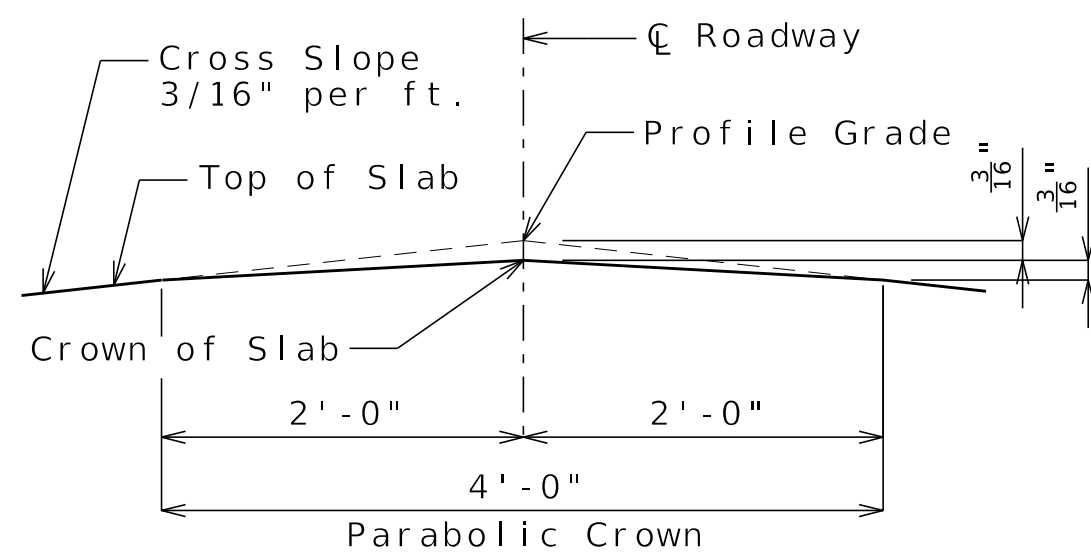


SLAB REPLACEMENT DETAIL

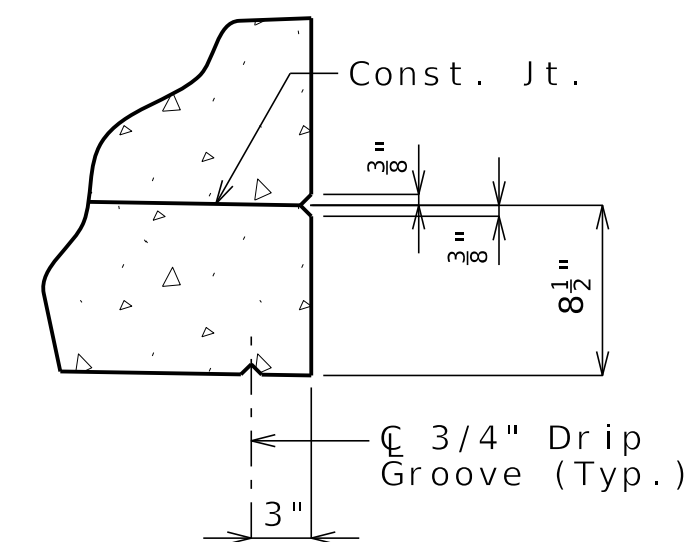
Note: Full depth slab replacement required over Girder and overhangs.



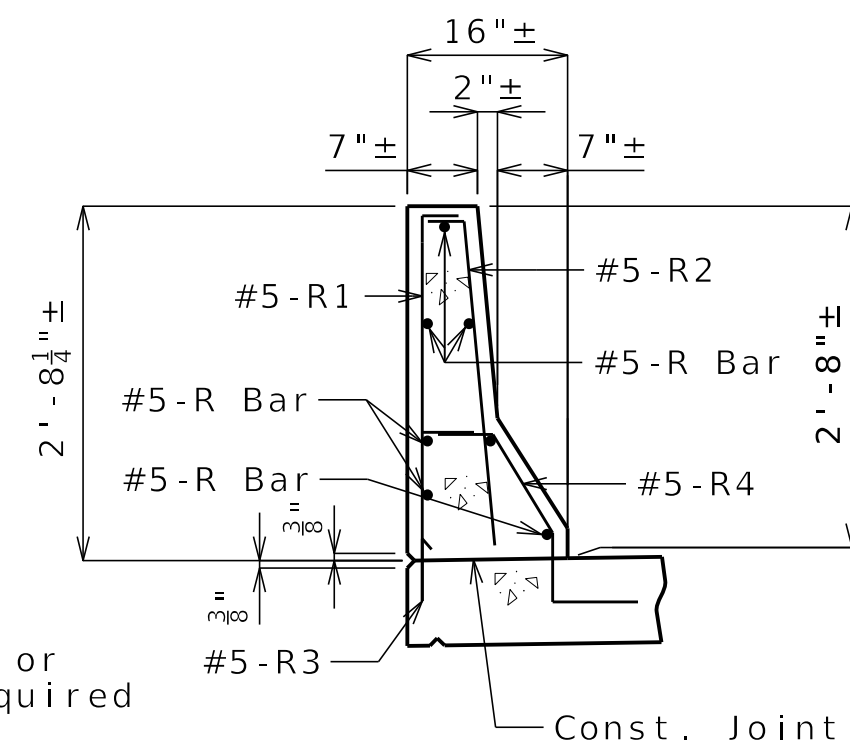
PART ELEVATION OF BARRIER



CROWN DETAIL



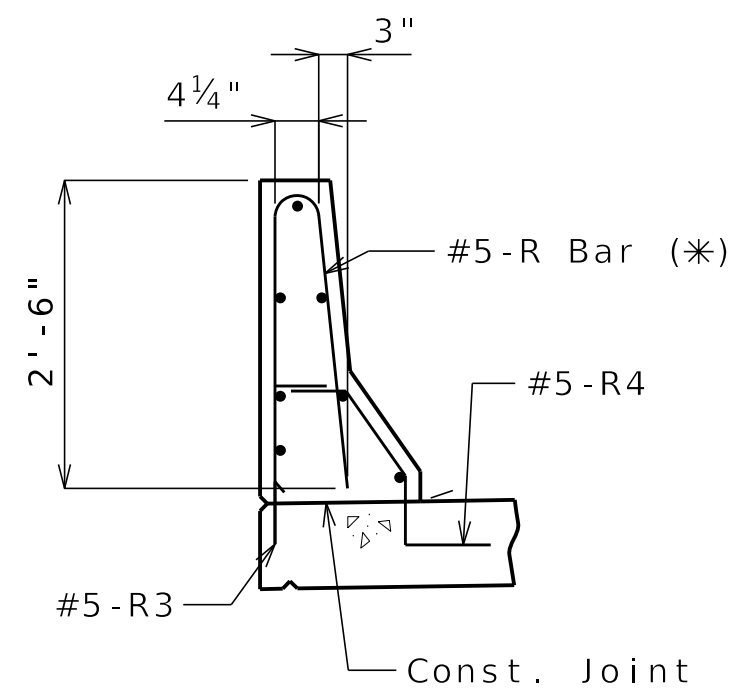
DRIP GROOVE DETAIL



SECTION A-A

The cross-sectional area above the slab is 2.27 square feet.

Cut or bend existing longitudinal #5-R bars & #5-R4 to clear segmental joint breakout by 2" as needed.



R-BAR PERMISSIBLE ALTERNATE SHAPE

(*) The R1 and R2 bar combination may be furnished as one bar, as shown, at the contractor's option. (All dimensions out to out.)

Notes:

Sections shown are taken at C Structure between girders. Contractor shall preserve girder reinforcing that extends into new concrete if not in conflict with the new joint placement.

Remove concrete in accordance with Sec 216.

Removal shall include removal of the existing expansion joint armor and anchorage as detailed.

Concrete removal to provide 1" minimum clearance beneath exposed reinforcing to remain. Removal Depth may be increased as needed to accommodate new anchorage requirements for the replacement joint as per the manufacturer's recommendations.

Any existing concrete traffic delineators on the barrier curb removed during the joint replacement process shall be replaced with like and kind.

Barrier curb to be built parallel to grade with barrier curb joints on superstructure.

Top of WaboFlex Joint shall be recessed 1/4" from the top of finished overlay.

All exposed edges of barrier curb (Type B) shall match existing barrier curb edges.

Existing Reinforcing may be replaced with like and in kind in accordance with Sec 216.

Any damage to the Existing Reinforcing Epoxy Coating shall be repaired according to Sec 710.

Concrete work shall be in accordance with Sec 704.

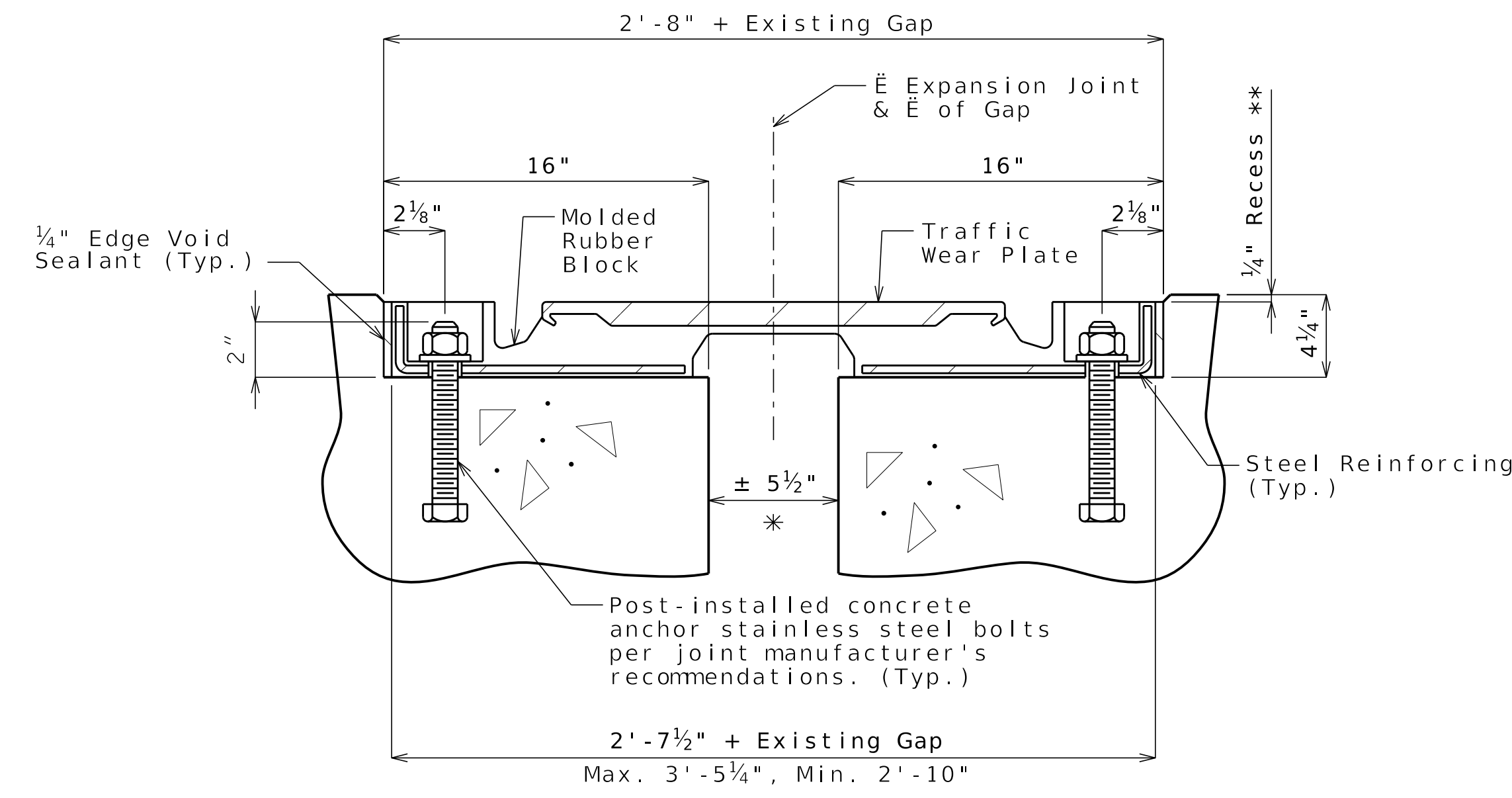
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JOB NO.
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CONTRACT ID.
PROJECT NO.
BRIDGE NO.
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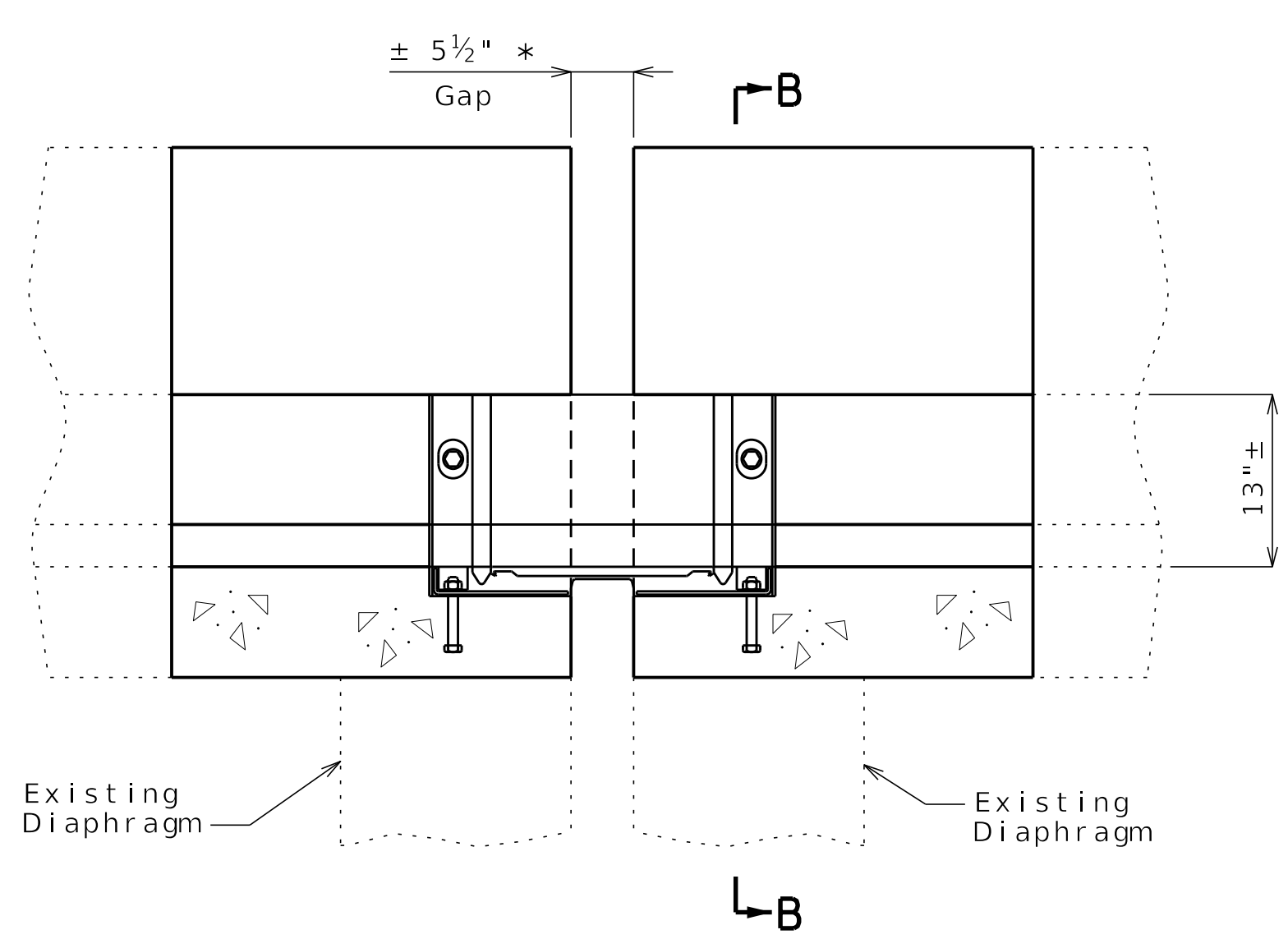
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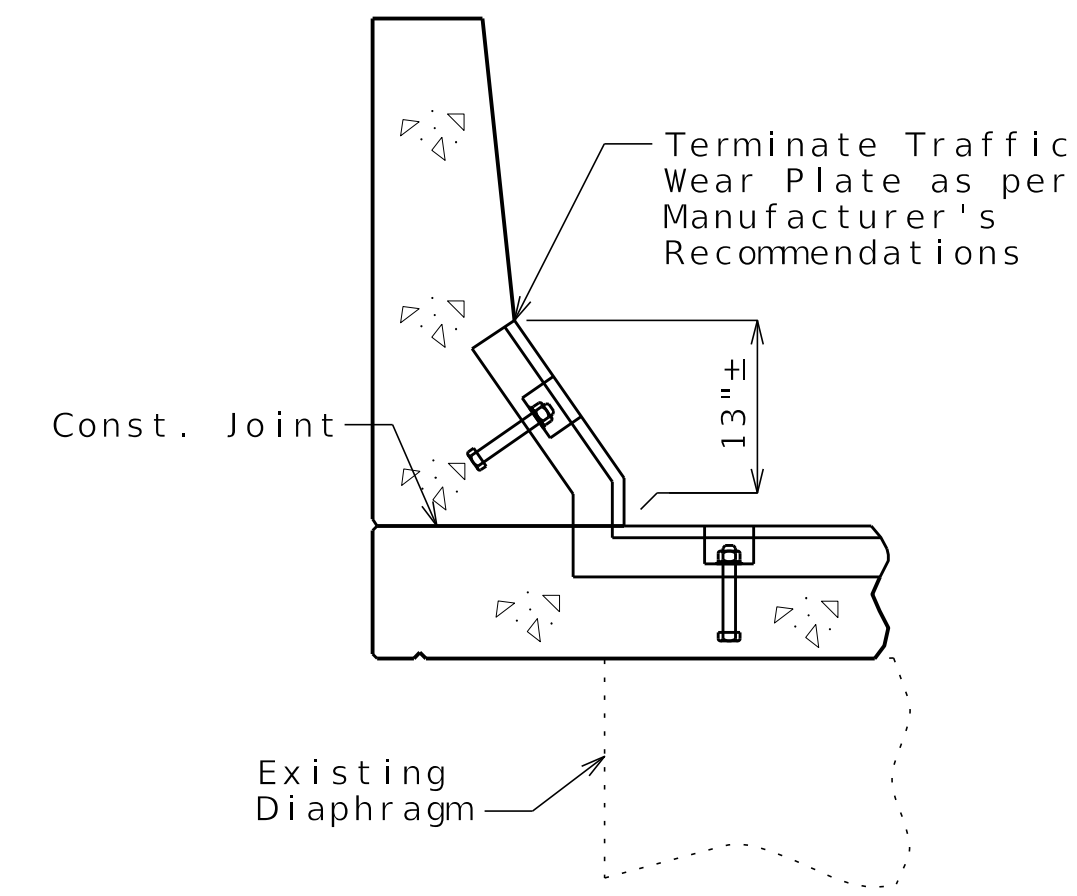
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SECTION THRU JOINT



PART ELEVATION OF BARRIER



SECTION B-B

* Existing plans provided 9" gap from face to face of concrete diaphragms at 60°F at time of construction. For replacement joint, reconstruct gap to 5 1/2" at 60° F.


Gap dimensions are based on installation at 60°F. The expansion gap and other dimensions shall be increased 1/2" for each 10° fall and decreased 1/2" for each 10° rise in temperature at installation.

Notes:

The installation temperature shall be taken as the actual air temperature averaged over the 24-hour period immediately preceding installation.

Replacement expansion joint shall be a WaboFlex Model SR9 as manufactured by Watson Bowman Acme Corp. Amherst New York. The new joint is to be installed per manufacturer's recommendations.

The expansion device shall be fabricated to match the crown of the roadway.



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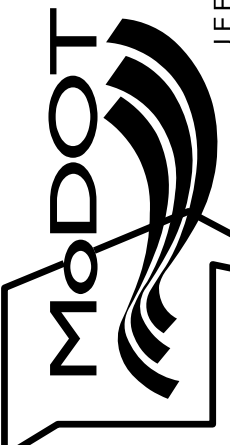
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DISTRICT BR	SHEET NO. A54062-14
COUNTY ST. CHARLES	
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BRIDGE NO. A54062	


DESCRIPTION	DATE

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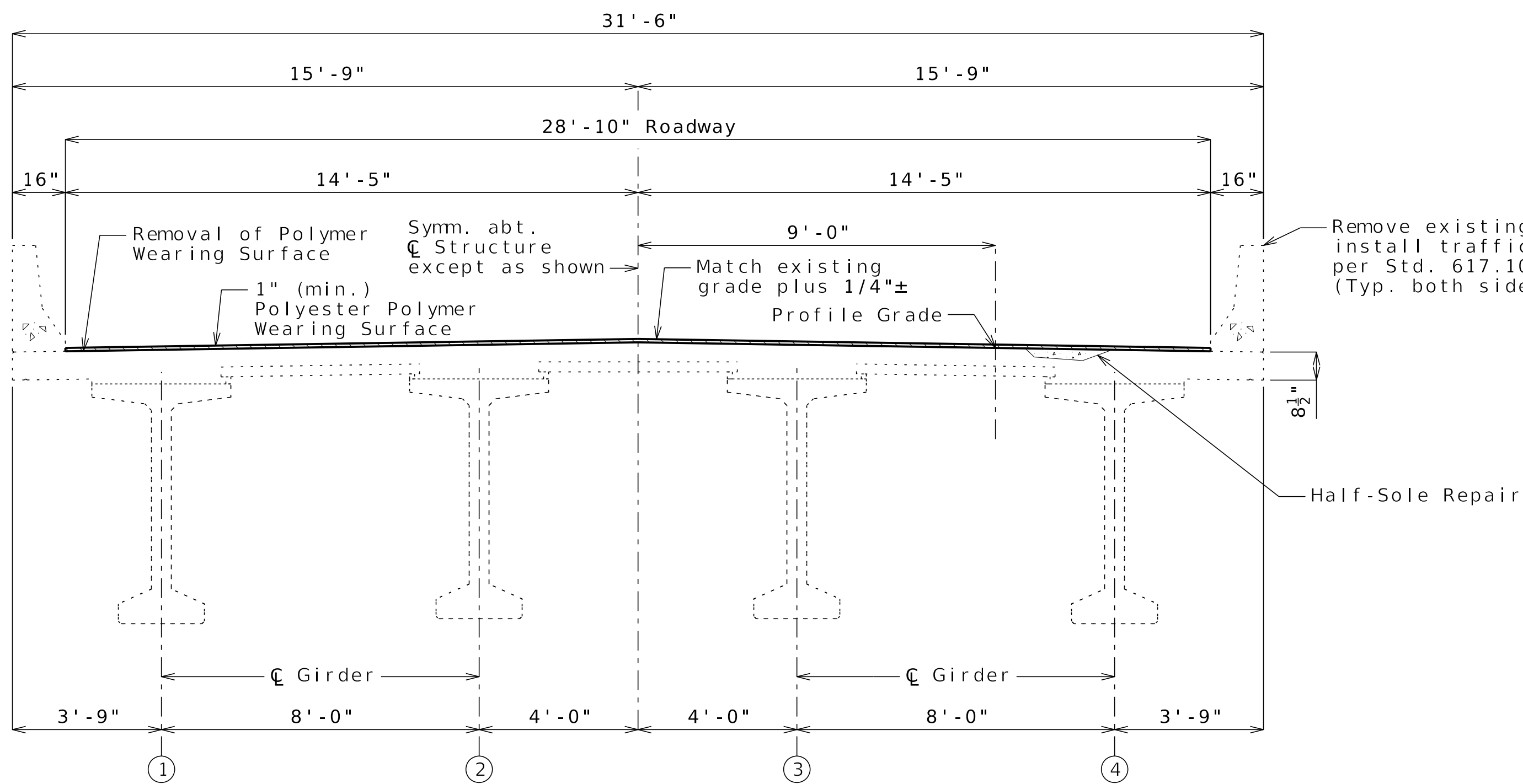




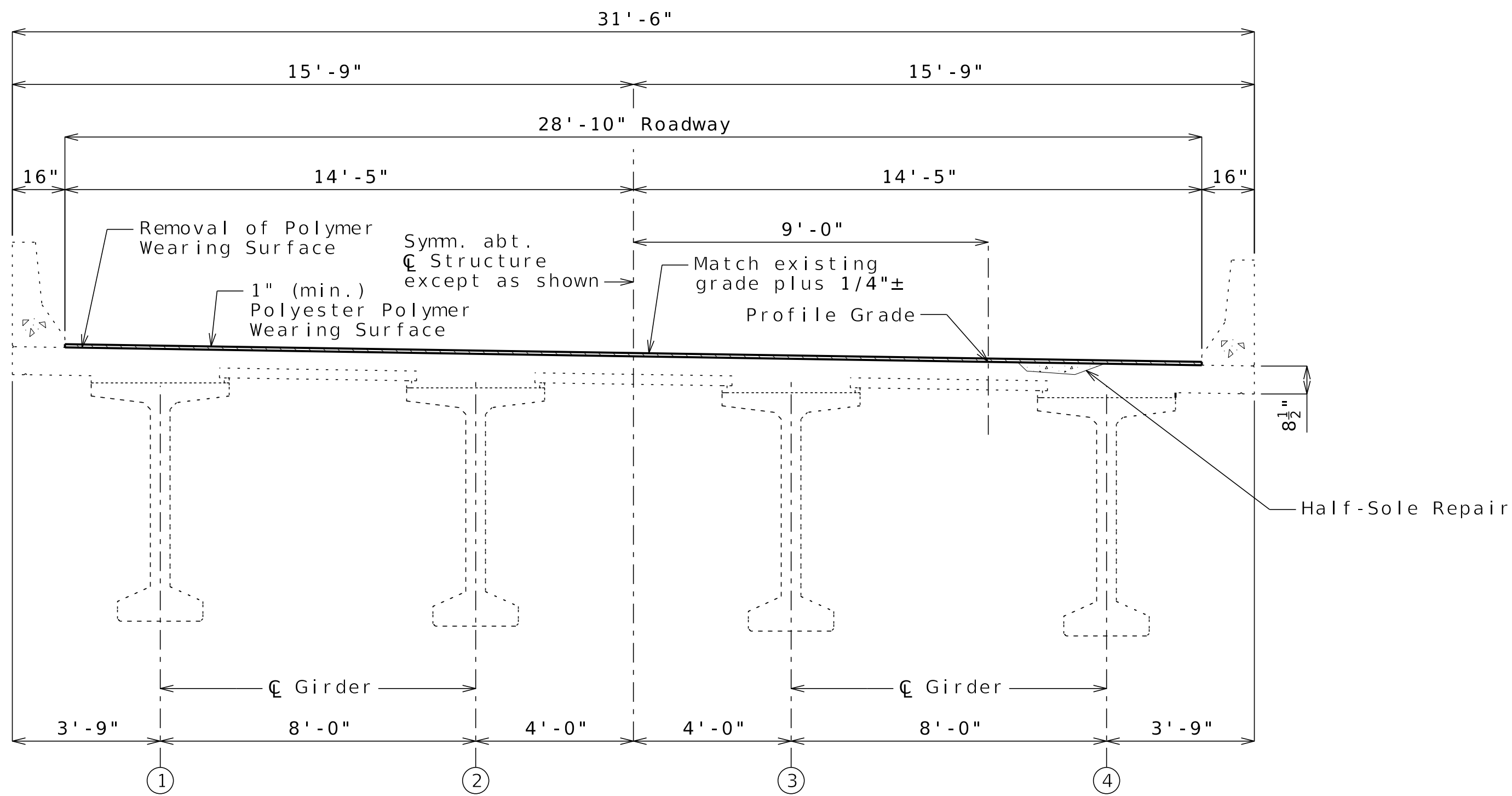
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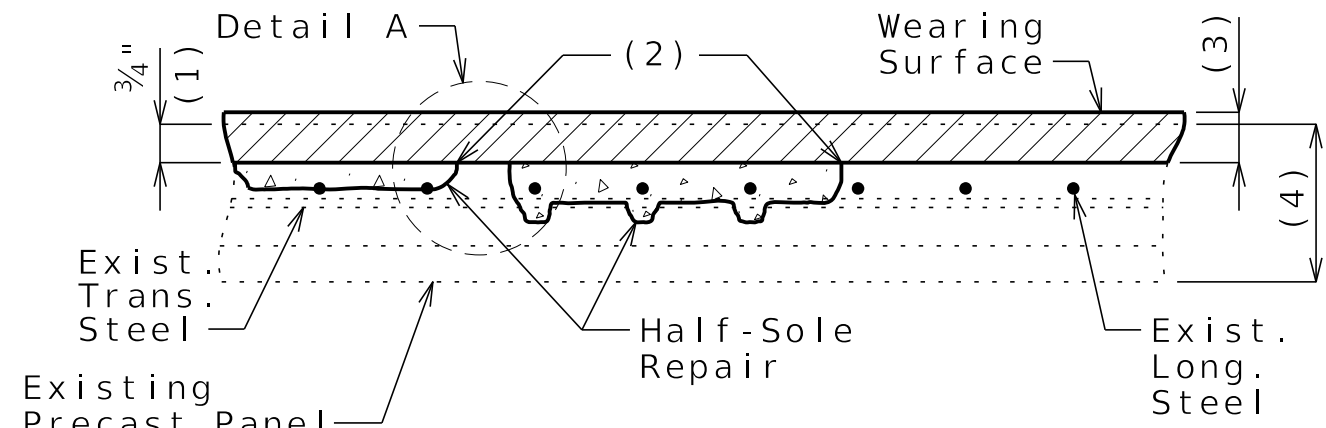
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TYPICAL TANGENT SECTION THRU SLAB

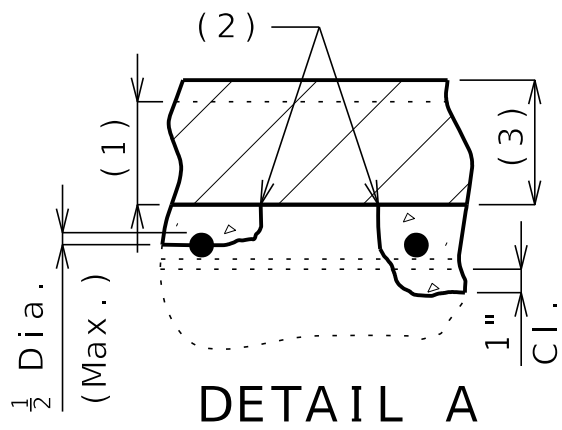


SUPERELEVATION SECTION THRU SLAB



HALF-SOLE REPAIR

Estimated Half-Sole Repairs=600 sq. ft.



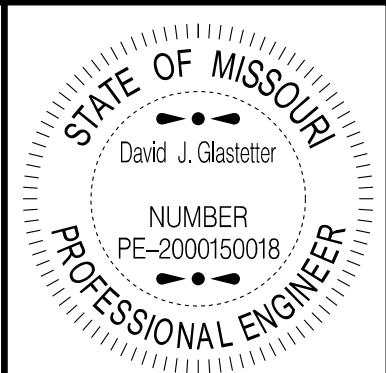
DETAIL A

Clearance around top bar and around bottom bar at the intersection of top bar shall be required when more than half the diameter of the top bar is exposed.

- (1) Remove Existing 1/4"± Epoxy Polymer wearing surface Plus 1/2" of existing deck.
(2) 1" vertical side shall be established outside the deteriorated area.
(3) 1" minimum Polyester Polymer wearing surface.
(4) Original depth of deck.

Notes:

Polyester Polymer Concrete may be substituted for Class B-2 Concrete at locations of half-sole repairs.



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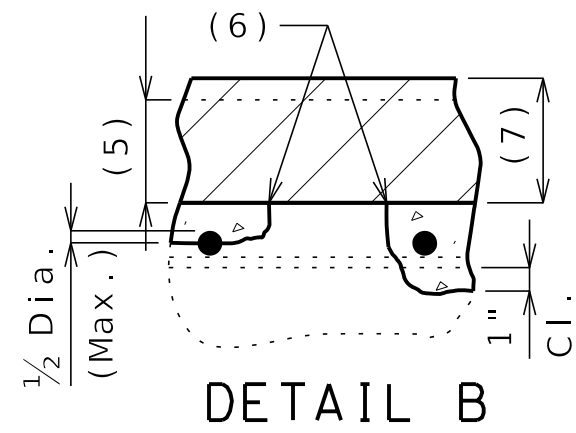
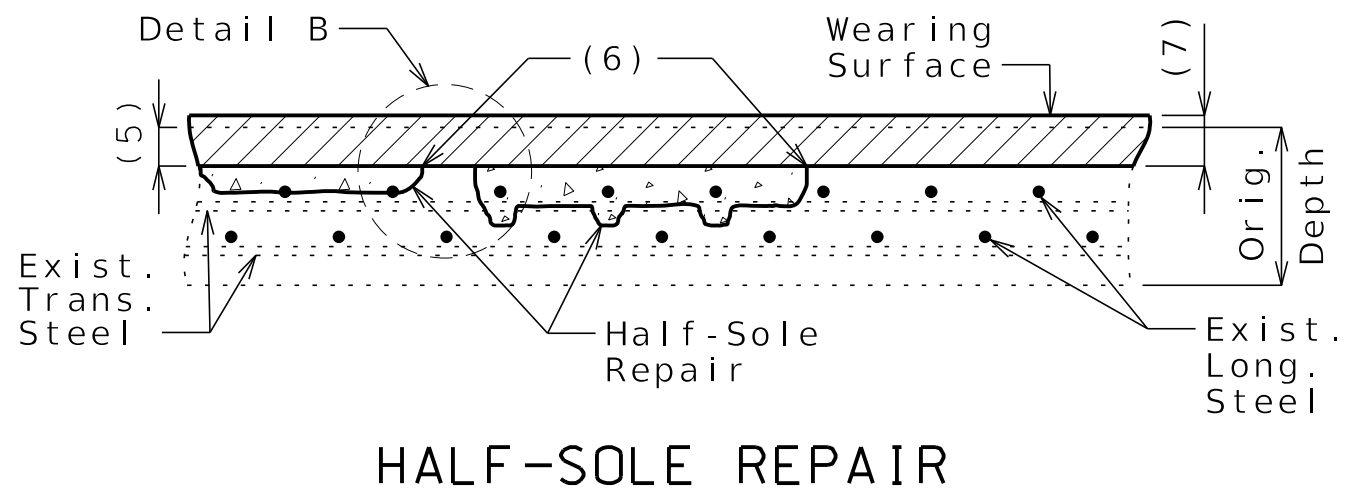
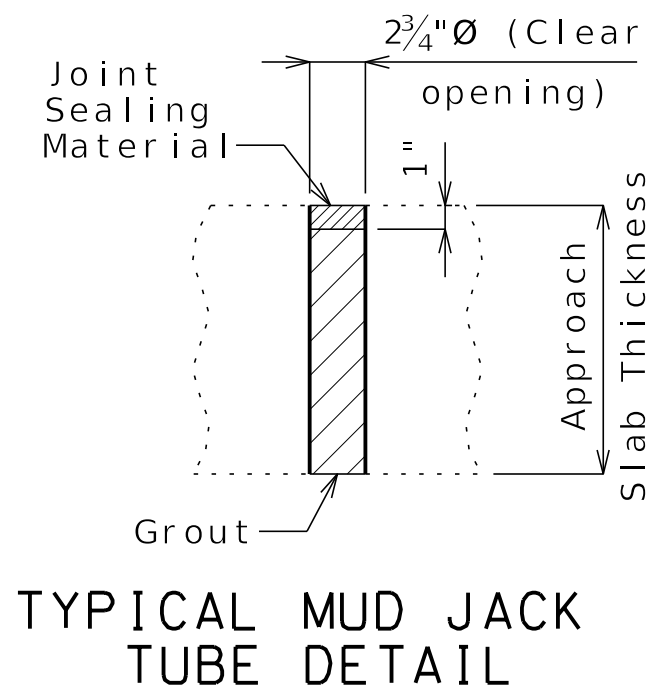
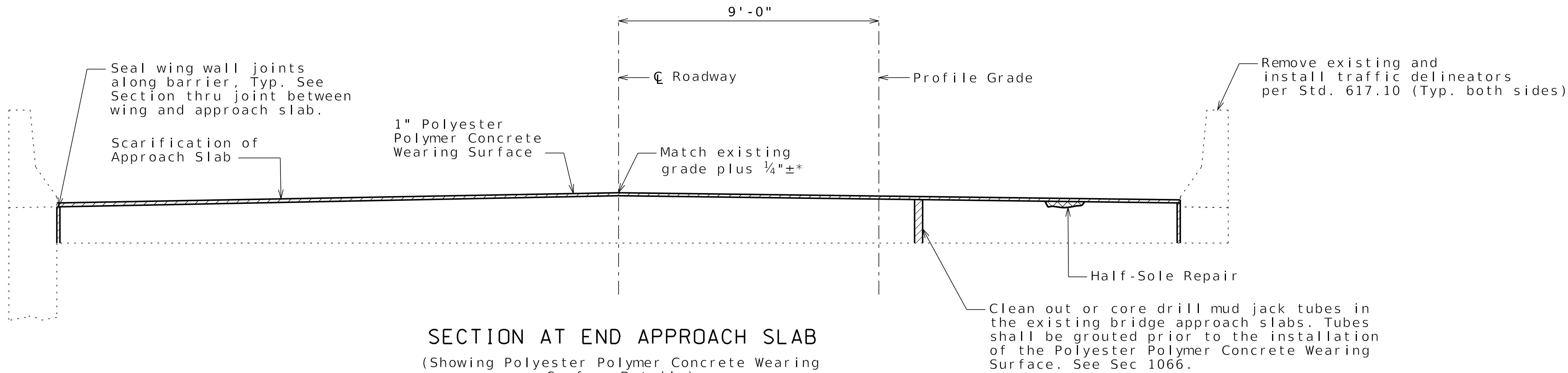
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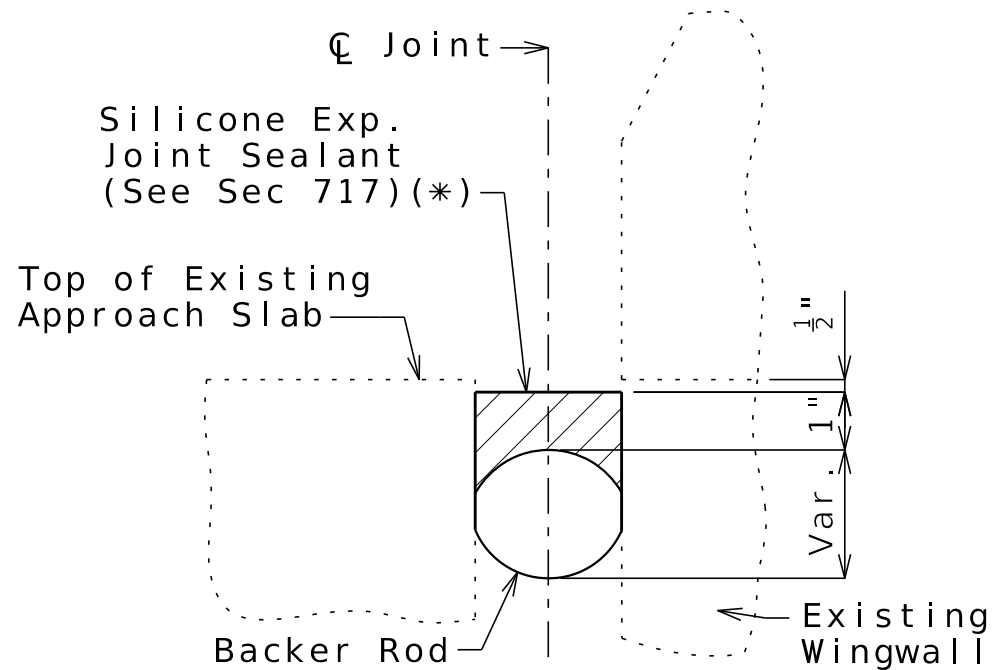


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- (5) 3/4" scarification of existing slab *
 - (6) 1" vertical side shall be established outside the deteriorated area.
 - (7) 1" minimum PPC wearing surface
- * Perform variable depth transition across bridge approach slab to match existing pavement elevation at outer edge of roadway approach pavement. Maintain constant depth of new wearing surface and vary scarification. Dimensions shown are at the interface with the bridge end bents.



* Seal joint between vertical face of approach slab and wing with "Silicone Joint Sealant for Saw Cut and Formed Joints" in accordance with Sec. 717.

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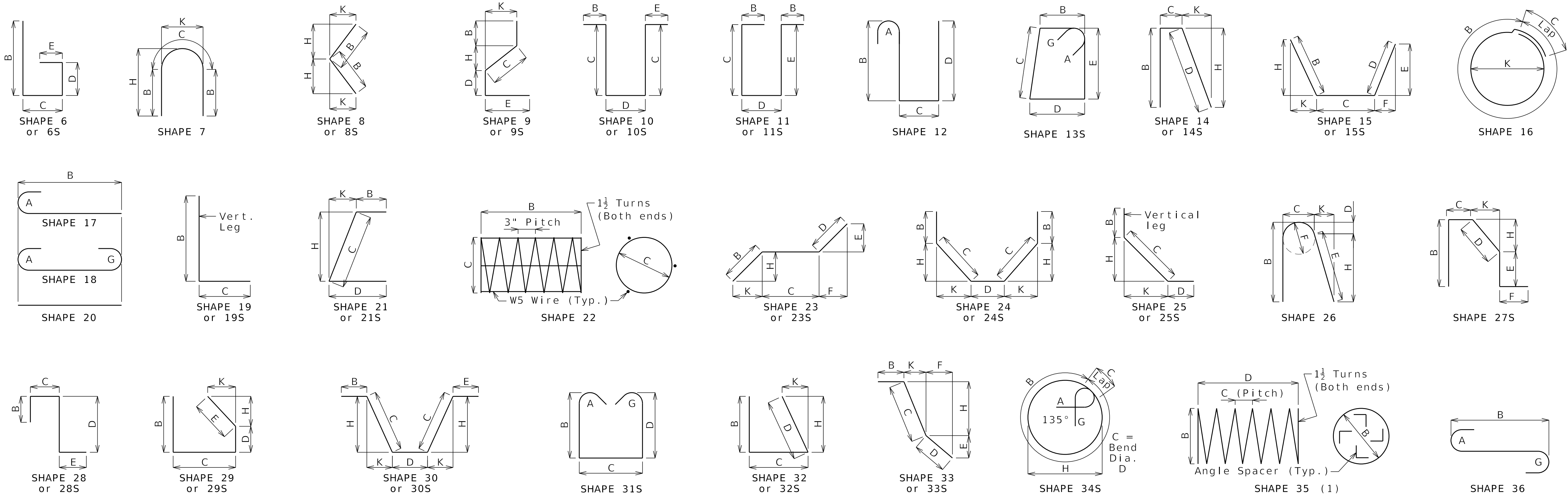
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Finished Bend Diameters D and Hook Dimensions							
Standard Pin Bend Shapes							
Size	Case	D	A or G		J		
			90°	180°	180°		
#4	1	3"	8"	6"	4"		
#5	1	3 3/4"	10"	7"	5"		
#6	1	4 1/2"	12"	8 1/4"	6"		
#7	2	5 1/4"	14"	9 3/4"	7"		
	3	7"	15"	11 1/2"	8 3/4"		
#8	2	6"	16"	11"	8"		
	3	8"	17"	13 1/4"	10"		
#9	1	9 1/2"	19 1/2"	15 1/2"	11 3/4"		
#10	1	10 3/4"	22"	17 1/2"	13 1/4"		
#11	1	12"	24 1/2"	19 1/2"	14 7/8"		
#14	1	18 1/4"	31 1/4"	27 1/2"	21 5/8"		
#18	1	24"	41 1/2"	36 1/4"	28 1/2"		

90°

180°

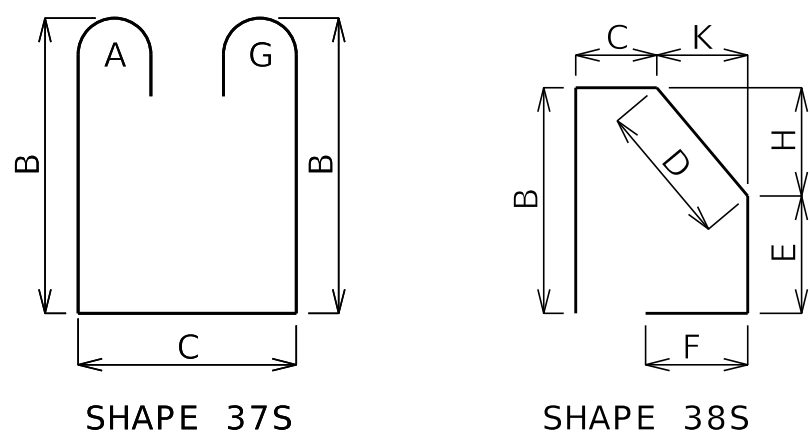
Stirrup Pin Bend Shapes (S)							
Size	Case	D	A or G		H		
			90°	135°	180°	135°	180°
#4	2	2"	4 1/2"	4 1/2"	5"	2 7/8"	3"
	3	3"	5"	5 1/4"	6"	3"	4"
#5	2	2 1/2"	5 3/4"	5 3/4"	5 3/4"	3 5/8"	3 3/4"
	3	3 3/4"	6 1/4"	6 1/4"	7"	3 5/8"	5"
#6	1	4 1/2"	12"	7 3/4"	8 1/4"	4 5/8"	6"

90°

135°

180°

Applicable for all grades of steel.
Case 1 applies to all reinforcement. Case 2 applies to all reinforcement except for galvanized bars. Case 3 applies to galvanized bars only.



BENDING DIAGRAMS

All dimensions are out to out.

(1) Shall be a deformed or plain spiral bar or wire.

Shapes ending with an S shall be bent in accordance with stirrup pin bend shapes.

Unless otherwise noted, finished bending diameter D is the same for all bends of a shape.

Four angle or channel spacers are required for each column spiral. Spacers are to be placed on inside of spirals. Length and weight of column spirals do not include splices or spacers.

Reinforcing Steel Totals (Pounds)								
Size	Substructure		Superstructure				Entire Bridge	
	Plain	Epoxy	Slab		Barrier	Slip Form	Plain	Epoxy
			Plain	Epoxy				
W5	0	0	0	0	0	0	0	0
4	0	0	0	0	0	0	0	0
5	0	0	0	63	472	0	0	535
6	0	0	0	1384	0	0	0	1384
7	0	0	0	0	0	0	0	0
8	0	0	0	0	0	0	0	0
9	0	0	0	0	0	0	0	0
10	0	0	0	0	0	0	0	0
11	0	0	0	0	0	0	0	0
14	0	0	0	0	0	0	0	0
18	0	0	0	0	0	0	0	0
By Type	0	0	0	1447	472	0	0	1919

All superstructure reinforcing steel shall be epoxy coated unless otherwise specified.

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06/10/2025
Released for Construction

<div><div>BENCHMARK 100 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'SIGN POST' (top), 'WATER VALVE' (top-right), 'NORTH EDGE OF ASPHALT ROUNDABOUT' (right), 'TOP END OF 24" R.C.P.' (bottom-left), and 'DELINEATOR POST' (bottom-right). Distances are labeled: 13.85', 25.82', 36.56', 12.64', 81.56', 46.74'.</p></div><div><p>NORTH = 1081423.367 EAST = 723104.330 ELEV.= 599.597</p></div></div>			<div><div>BENCHMARK 113 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'HANDHOLE' (top-right), 'GAS VALVE' (right), 'TELEPHONE PEDESTAL' (bottom), and 'END OF GUARDRAIL' (bottom-left). Distances are labeled: 15.63', 8.92', 28.64', 8.93'.</p></div><div><p>NORTH = 1078977.686 EAST= 728186.747 ELEV.= 623.007</p></div></div>	<div><div>BENCHMARK 114 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'SOUTH OF ASPHALT ROAD' (top), 'MAG NAIL IN UTILITY POLE' (top-right), 'R.O.W. MARKER' (bottom), 'MAG NAIL IN UTILITY POLE' (bottom-right), 'EDGE OF ASPHALT ROAD' (left), and 'STEEL POWER POLE' (bottom-left). Distances are labeled: 7.06', 67.06', 33.91', 22.61', 8.06', 49.12', 2.77'.</p></div><div><p>NORTH = 1082531.641 EAST= 727818.620 ELEV.= 633.117</p></div></div>	<div><div>BENCHMARK 115 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'SOUTH EDGE OF ASPHALT ROAD' (top), 'WATER VALVE' (top-right), 'SANITARY MANHOLE LID' (bottom-right), and 'END OF GUARDRAIL' (bottom-left). Distances are labeled: 10.29', 46.55', 45.03'.</p></div><div><p>NORTH = 1082444.293 EAST= 729392.876 ELEV.= 619.994</p></div></div>	
<div><div>BENCHMARK 116 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'END OF GUARDRAIL' (top-right), 'NW CORNER OF INLET' (right), 'DELINEATOR POST' (bottom-right), and 'MAG NAIL IN FOGLINE' (bottom-left). Distances are labeled: 99.61', 46.74', 81.56', 21.21', 1.43', 35.59'.</p></div><div><p>NORTH = 1082653.370 EAST = 723222.089 ELEV.= 605.972</p></div></div>	<div><div>BENCHMARK 200 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'GAS VALVE' (top-right), 'TELEPHONE PEDESTAL' (left), 'END OF GUARDRAIL' (bottom-left), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 16.50', 8.93', 26.64', 35.16', 36.66'.</p></div><div><p>NORTH = 1078971 EAST= 728573 ELEV.= 613.97</p></div></div>	<div><div>BENCHMARK 208 75 BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'MAG NAIL IN FOGLINE' (top-right), 'END OF GUARDRAIL' (left), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 25.36', 35.16', 36.66'.</p></div><div><p>NORTH = 1082525 EAST= 721080 ELEV.= 597.57</p></div></div>	<div><div>BENCHMARK 209 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'DELINEATOR POST' (top-right), 'MODOT TRAFFIC SIGNAL HANDHOLE' (bottom-right), 'STEEL POWER POLE' (bottom-left), and 'MAG NAIL IN FOGLINE' (top-left). Distances are labeled: 8.06', 49.12', 2.77'.</p></div><div><p>NORTH = 1082558 EAST= 721763 ELEV.= 599.72</p></div></div>	<div><div>BENCHMARK 210 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN TOP OF GUARD POST' (top), 'MAG NAIL IN FOGLINE' (top-right), 'MAG NAIL IN FOGLINE' (right), 'MAG NAIL IN FOGLINE' (bottom-right), 'HANDHOLE' (bottom-left), and 'MAG NAIL IN FOGLINE' (left). Distances are labeled: 1.43', 35.59', 47.15', 55.27', 47.66'.</p></div><div><p>NORTH = 1082628.386 EAST= 722867.362 ELEV.= 604.348</p></div></div>	<div><div>BENCHMARK 211 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'MAG NAIL IN FOGLINE' (top-right), 'HANDHOLE' (bottom-left), and 'MAG NAIL IN FOGLINE' (left). Distances are labeled: 47.15', 55.27', 47.66'.</p></div><div><p>NORTH = 1082667 EAST= 723955 ELEV.= 602.59</p></div></div>	
<div><div>BENCHMARK 212 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'DELINEATOR POST' (top-right), 'MILE MARKER SIGN' (left), and 'DELINEATOR POST' (bottom-right). Distances are labeled: 29.27', 134.28', 59.52'.</p></div><div><p>NORTH = 1082716 EAST= 724964 ELEV.= 610.96</p></div></div>	<div><div>BENCHMARK 213 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'SOUTH EDGE OF ASPHALT ROAD' (top-right), 'MAG NAIL IN FOGLINE' (right), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 2.23', 28.03', 28.01'.</p></div><div><p>NORTH = 1082709 EAST= 726010 ELEV.= 622.86</p></div></div>	<div><div>BENCHMARK 214 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'DELINEATOR POST' (top-right), 'MAG NAIL IN FOGLINE' (right), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 23.60', 61.55', 28.60'.</p></div><div><p>NORTH = 1082643 EAST= 727148 ELEV.= 631.52</p></div></div>	<div><div>BENCHMARK 215 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'SOUTH EDGE OF ASPHALT ROAD' (top), 'TELEPHONE PEDESTAL' (top-right), 'UTILITY POLE' (left), and 'TELEPHONE PEDESTAL' (bottom-right). Distances are labeled: 18.25', 62.86', 69.92'.</p></div><div><p>NORTH = 1082500.421 EAST= 728168.824 ELEV.= 626.184</p></div></div>	<div><div>BENCHMARK 216 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'TELEPHONE PEDESTAL' (top), 'NW CORNER OF RETAINING WALL' (top-right), 'GAS METER' (right), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 1.68', 21.61', 27.91'.</p></div><div><p>NORTH = 1082415 EAST= 729223 ELEV.= 616.20</p></div></div>	<div><div>BENCHMARK 217 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'TELEPHONE MARKER' (top), 'TOP BACK OF CURB FOR PARKING LOT' (top-right), 'GAS LINE MARKER' (right), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 8.99', 17.87', 56.21', 39.12'.</p></div><div><p>NORTH = 1082396 EAST= 730135 ELEV.= 612.21</p></div></div>	
	<div><div>BENCHMARK 220 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'WATER VALVE' (top), 'FIRE HYDRANT' (top-right), 'STORM MANHOLE LID' (bottom-left), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 11.37', 24.50', 16.22', 37.14'.</p></div><div><p>NORTH = 1080588 EAST= 726887 ELEV.= 602.07</p></div></div>		<div><div>BENCHMARK 228 75 -BM 2IN SQ CUT</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'SW CORNER OF STORM GRATE' (top-right), 'MAG NAIL IN FOGLINE' (right), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 12.34', 36.44', 37.14'.</p></div><div><p>NORTH = 1084862 EAST= 722908 ELEV.= 656.76</p></div></div>	<div><div>BENCHMARK 229 74 /5/8x24_ROD_W/CAP</div><div><p>Diagram showing a circle with a center point. Lines radiate from the center to various points: 'MAG NAIL IN FOGLINE' (top), 'END OF GUARDRAIL' (top-right), 'MAG NAIL IN FOGLINE' (right), and 'MAG NAIL IN FOGLINE' (bottom-right). Distances are labeled: 17.55', 39.12', 68.70'.</p></div><div><p>NORTH = 1083775.980 EAST= 723240.823 ELEV.= 619.275</p></div></div>		

Detailed JUN 2025
Checked JUN 2025

Note: This drawing is not to scale. Follow dimensions.

Sheet 19 of 20

SURVEY CONTROL POINTS

STATE OF MISSOURI
David J. Glasstetter
NUMBER
PE-2000150018
PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN
SIGNED, SEALED AND DATED
ELECTRONICALLY.

DATE PREPARED
6/6/2025

ROUTE
I - 70

STATE
MO

DISTRICT
BR

SHEET NO.
A54062 - 19

COUNTY
ST. CHARLES

JOB NO.
JST0020

CONTRACT ID.

PROJECT NO.

BRIDGE NO.
A54062

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

HNTB
715 KIRK DRIVE
KANSAS CITY, MO 64105-1310
CERTIFICATE OF AUTHORITY
NO. 001270

Bartlett & West
601 MONROE ST, SUITE 201 - JEFFERSON CITY, MO 65101
PHONE 8724303181
ENGINEERING
CERTIFICATE OF AUTHORITY
WWW.BARTLETTWEST.COM

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